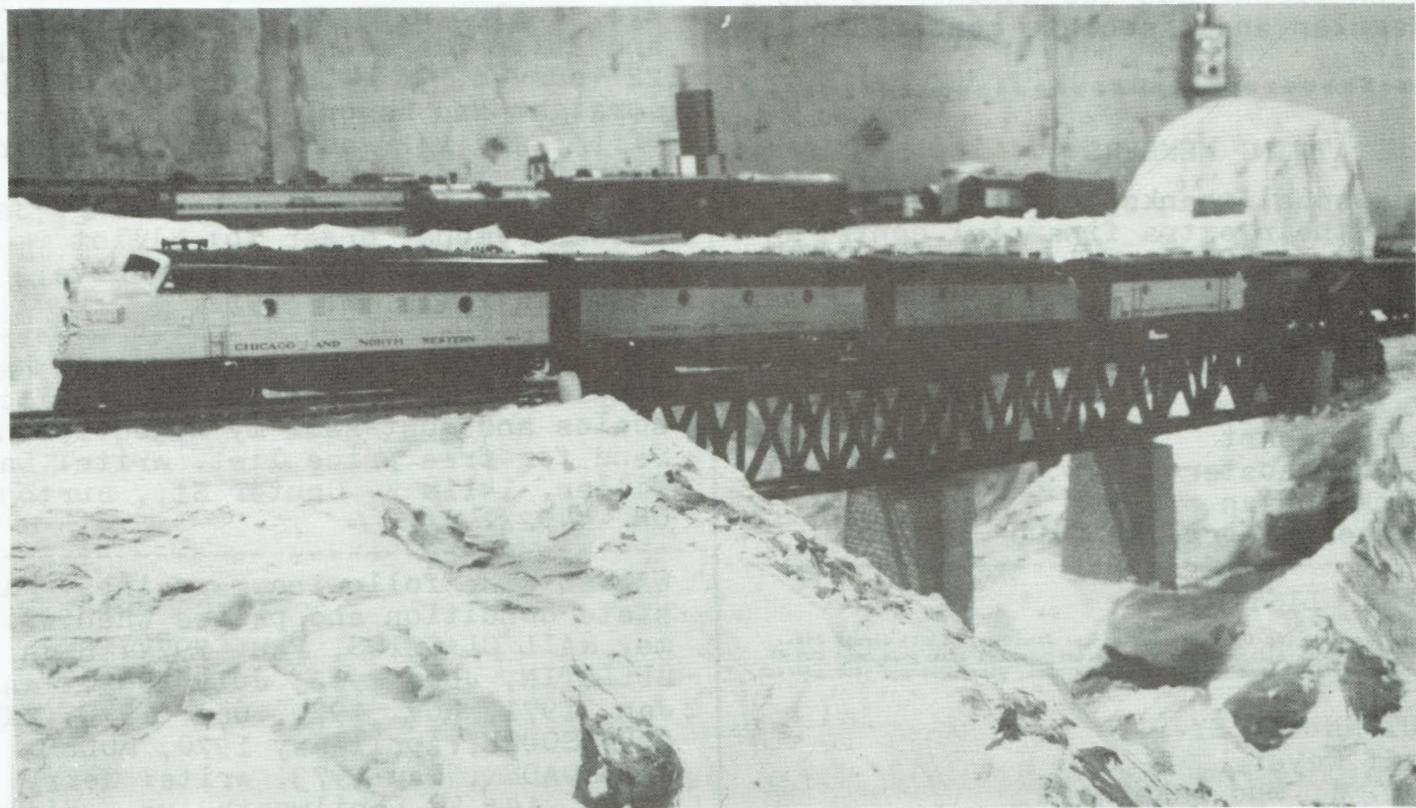




# HOTBOX

"the Un-Magazine of Model Railroading"  
September - October 1980      No. 163

## BUILDING



## BRIDGES



# HOTBOX

## OFFICIAL PUBLICATION\* Teen Association of Model Railroading

Issued every other month with an additional special mailing of a Directory of Membership during the summer.

Annual dues for the TAMR are as follows:

REGULAR (under 21 years of age): \$5.00

ASSOCIATE (21 years of age and up): \$4.50

SUSTAINING (both Regular & Associate): \$10.00

Please address all membership applications, renewals, address changes and complaints of non-receipt of the TAMR HOTBOX to the TAMR Secretary.

Gerry Dobey, TAMR Secretary  
145 E. Kenilworth Ave.  
Villa Park, IL 60181

All other HOTBOX business, except where specifically noted, is handled by the Editor. Please address all comments to the Editor.

**DEADLINE:** All material for publication must be submitted for consideration by the first day of the first month of the issue (i.e. for May/June issue, material must be received no later than May 1st). The TAMR HOTBOX assumes all material is contributed gratis and no payment will be made upon publication.

Mark Kaszniak, HOTBOX Editor  
4818 W. George Street  
Chicago, IL 60641

\*\*\*\*\*  
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## SUSTAINING MEMBERS:

A special thanks to the following for their extra financial contribution to the TAMR.

Jeff Meyer  
Steve Kahl  
Steve Bockbrader  
Talmadge Carr  
Dee Gilbert  
A.G. Schuster  
Mark Morgan  
Newberry Model RR Club

## PASS LISTING:

Send your request for the Pass Listing to: John Huey, 13819 Jersey Ave., Norwalk, CA 90650. All requests must be accompanied by a pass made out to John and a 3x5 card showing your railroad name, your name and your address PRINTED or TYPED.

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Dubuque, IA 52001

SOUTHFORK & BLACK RIVER  
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## INTERCHANGE

If you have something to Buy, Sell or Trade, use the INTERCHANGE to get results. Your ad is seen by all TAMR members. Rate: 10¢ per column line (35 spaces), your name and address printed FREE. Send all ads to: Richard Sonoski, 219 First Street, Fort Carbon, PA 17965. Rich will also accept ads for your pike too. Rate: 20¢ per column line, name and address printed FREE. If you want your herald or other artwork reproduced, the rate is 50¢ per square column inch. Send in those ads today and make your HOTBOX better tomorrow.

**WANTED:** Used HO N3 freight cars. Please send list and include price, condition, etc. Write: Andy Cornwall, 13733 Wabash Ave., Riverdale, IL 60627

**WANTED:** Lionel #412, 413, 414 and 416 passenger cars in Excellent condition. Will pay \$4,000 for matching set in green or brown. Contact: Anthony Cassamassima, 62-48 84 Street, Middle Village, NY 13379

**FOR SALE:** 13 Atlas and Tyco track switches. All are brand new and include switch machines. Four Tyco R.H., four L.H., two Atlas custom line #6 R.H., 1 Atlas custom line #6 L.H. and 1 Atlas Wye. I also have new and used Tyco and Atlas track. 8 - 6" straight, 16 - 22" curved, 36 - 18" curved, 35 - 9" straight and assorted small sections of 1/3 and 1/2 radius curves. Entire pack for \$45.00 or best offer. No charge for postage and handling. Send check or money order to: Elliot Nowacky, 780 Hidden Valley Drive, Columbia Falls, MT 59912.

**FOR SALE:** Track, switches, cars, kadee couplers and many parts. I am switching scales and must sell my HO equipment. Send for free price list. Write: Dale Miller, 14118 E. Center St., Burton, OH 44021.

**WANTED:** The following magazines. Please state condition and prices when writing me. RAIL CLASSICS, Feb. 1974; NMRA BULLETIN, July 1972, July 1969; TRAINS, Jan. 1971, Nov. 1970, Oct. 1970; EXTRA 2200 SOUTH, April-May 1970, MODEL RAILROADER, May 1973. write: Gerry Dobey, 145 E. Kenilworth Ave., Villa Park, IL 60181.

INTERCHANGE is continued on page 5.

# CRUMMY NEWS



BY MARK KASZNAIK, EDITOR

## GROWING PAINS

As every association expands, it experiences a phenomenon known technically as "growing pains." The problem is that the association wants to expand so it can provide more and better benefits to its members, but finds itself limited by the amount of money it has to spend coupled with the talent of its officials. Sooner or later, the plain ordinary member (POM) starts to complain and this leaves the administration running the show one of two options. The officials can give up or they can listen to the members complaints and try to make improvements in the areas where the members feel that they are obviously needed.

Back in 1973-74, the officials of this association decided to give up and general chaos resulted. The TAMR is growing very close to that situation once more and in an effort to head off the impending doom, I am bringing a number of criticisms leveled against the TAMR out in the open for discussion and comment. Let's hope the final course taken this time is different from that taken seven years ago.

1) WHY DOES THE TAMR SECRETARY TAKE SO LONG TO PROCESS MY APPLICATION ONCE I'VE SENT IN MY DUES MONEY?

This problem is related directly to the publication of the HOTBOX. In order to save the TAMR money, your renewal notice and membership card are sent inside your HOTBOX. So naturally if you renew when you're supposed to (i.e. when you see

the notice inside your HOTBOX), you won't receive your membership card until the next mailing of the HOTBOX. This is the procedure that was set up by the Secretary and approved by the Executive Board. However, there are times when you may receive your card sooner. For instance, you may write the Secretary a letter about some facet of the TAMR or maybe you've decided to order one of our badges or buttons. In this case, since the Secretary has to respond to your request anyway, he will most likely include your membership card in his response.

For new members, the procedure is a little different. When you join the TAMR, the Secretary normally sends out your membership card, a copy of our Directory and Constitution plus the latest issue of the HOTBOX. Yet there are times when we run out of the HOTBOX, our Constitution and our Directory so you might have to wait until the next issue of the HOTBOX is mailed before you receive your membership card.

2) WHY DOES ALL THE ACTIVITY IN THE TAMR SEEM TO CENTER AROUND THE HOTBOX AND REGIONAL NEWSLETTERS?

This is a good question and it has been asked several times in the past by our officers. The answer has to deal with the amount of money, time and transportation each TAMR member has at his/her disposal. Most teens have a limited amount of money to spend on their hobby and when they do spend it, they'd rather spend it on modeling/railfanning items rather than train/plane/bus/motel costs that are often associated with attending lots of conventions. Time is also another important factor. Almost all of our members are attending school and many have other interests besides modeling and/or railfanning. Couple these reasons with the fact that the TAMR has no permanent place to meet in any of its regions, divisions or what not and you can readily see why most of our activity is centered around our publications.

However, I do think that our regions must start planning more meets and conventions for the benefit of our members, especially in large metropolitan areas where transportation is inexpensive and available. These

(cont'd next page)

need not be elaborate affairs, often all that is needed is a free Saturday or Sunday afternoon and a place to meet. The convenience of your telephone coupled with our Directory can tell you who's available and if they can get to the meeting place.

Yet the subject of an optimum meeting place is all together a different matter. Over the years, the TAMR has advocated that our members get together at one another's houses on sort of a round robin basis and reimburse the host for any costs incurred (food, broken equipment, etc.). In large urban areas, this may not be the best idea as several members may live in apartments and thus the noise and general mayhem of such a meeting may tend to disrupt other family activities. In this case, it may be advisable to check out your local library, fieldhouse, YMCA, school or veteran's lodge. One of these places is likely to let you have a room for the afternoon (especially if your parents and/or you are actively involved). Just remember to conduct yourself in a civilized manner and clean up before you leave. Making a good impression the first time will make it easier to get the same facility the next time you need it.

### 3) WHY IS THE HOTBOX ISSUED ONLY SIX TIMES A YEAR AND NOT MONTHLY?

I often ask myself the same question and unfortunately I always arrive to the same conclusions. Let's be realistic, your HOTBOX costs us money, lots of it. Nearly 80% of your dues money goes to supporting this publication. We do not get the HOTBOX printed for cost of materials alone nor do we have any millionaire members who are going to donate large sums of money so we can go monthly. To get a monthly HOTBOX, you'd have to just about double our present dues or triple our membership. In addition, even if we could afford a monthly publication, I seriously doubt whether there'd be enough articles to fill it. I'm not saying that we won't provide you with larger issues if articles and finances are available and I'm sure if we get to the point where we are sending out 24 page HOTBOXes on a regular basis, our

officers would vote to go monthly.

### 4) WHAT CAN I DO IF I GET NO RESPONSE FROM THE PERSON WHO'S SUPPOSED TO HELP ME IN THE MEMBER SERVICES COMMITTEE?

If you do not receive a response after waiting a reasonable period of time, say two weeks, you should contact the chairman, Dee Gilbert. He will then take the necessary steps to see you get the help you need. The MSC is a voluntary service which means those who offer their services are doing so because they want to help others who are having problems. Unfortunately, there are usually a few who find that they can't handle the situation and instead of admitting this, they simply don't bother answering letters sent to them. It is the chairman's responsibility to see that everything is running smoothly and help is getting to those who need it.

### 5) WHAT DO I DO IF MY REGION NEWSLETTER DOESN'T COME OUT ON TIME?

In this case, write the editor of the newsletter and ask him about the delay. Your officers, the HOTBOX Staff or the MSC probably won't be able to help you. Sometimes a publication is late because of lack of articles, a sudden loss of printing facilities or someone just plain screwed up. This happens to the best of us and occasionally other things must come before your region newsletter or the TAMR. Try to understand, be patient and offer your help if it is needed.

I hope the airing of these criticisms and their explanation from my view serve to clear the air and promote a better understanding among our members, officials and the HOTBOX. Needless to say, these pages are always open to anyone who encounters any type of problem in the TAMR.

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NOTICE: The printing of this year's DIRECTORY has been pushed back to late Fall in an effort to find some way of producing it economically. Our mimeograph machine broke down and has yet to be repaired, if it can be. Anyone who has suggestions on this subject should let either the Secretary or myself know about them.

## RIO GRANDE, SANTA FE & PACIFIC RAILWAY

The Rio Grande, Santa Fe and Pacific Railway had its beginnings in central New Mexico in 1895 when the Santa Fe, San Juan and Northern started construction of a line from an AT&SF connection at Albuquerque, NM to Salt Lake City, UT. This line went into receivership in 1897 and was subsequently reorganized as the Rio Grande Northwestern (RGNW) RR.

In 1900, the line was completed to Salt Lake City and work was then started on a line to the Pacific. By 1924, the railroad had reached Reno, NV (see map). In 1926, the RGNW bought out the New Mexico Central (NMC) RR which gave it access to the Pecos Valley. Also in 1926, the railroad took control of the Rio Grande Eastern RY in central New Mexico.

In 1930, the Rio Grande Northwestern was renamed the Rio Grande, Santa Fe and Pacific RR after the Rio Grande, Texas and Mexico (RGT & M) and the Texas-New Mexico (T-NM) merged into the system. In 1933, the Midland Valley (MV) RR and the Kansas, Oklahoma & Gulf (KO&G) RR merged into the Rio Grande, Santa Fe and Pacific RR.

Next the RG, SF & P bought up 70% of the Santa Fe Northwestern RR's stocks and in 1937, the Sacramento Northern (SN) RR came under the control of the Rio Grande, Santa Fe and Pacific followed in 1940 by the Texas-Mexican (T-M) RR.

In the period between 1940 and 1955, the railroad did moderately well. Yet in 1955, a new management brought bankruptcy to the line. Thus the Sacramento Northern was sold to the Western Pacific. Then in 1956, the Reno-Marysville line was abandoned and in 1960, the narrow gauge Santa Fe Northwestern RR was also abandoned.

Finally, in 1960 the railroad was once again reorganized this time under the name of Rio Grande, Santa Fe and Pacific Railway. Joint passenger train operation began in conjunction with the Chicago, St. Louis & Wichita. However, by 1967, the rail line was once again on the verge of going bankrupt and the former Midland Valley was sold to the Texas & Pacific RR.

(E.N. - A map of the Rio Grande, Santa Fe and Pacific Ry appears on the next page.)

### INTERCHANGE (cont'd from page 2)

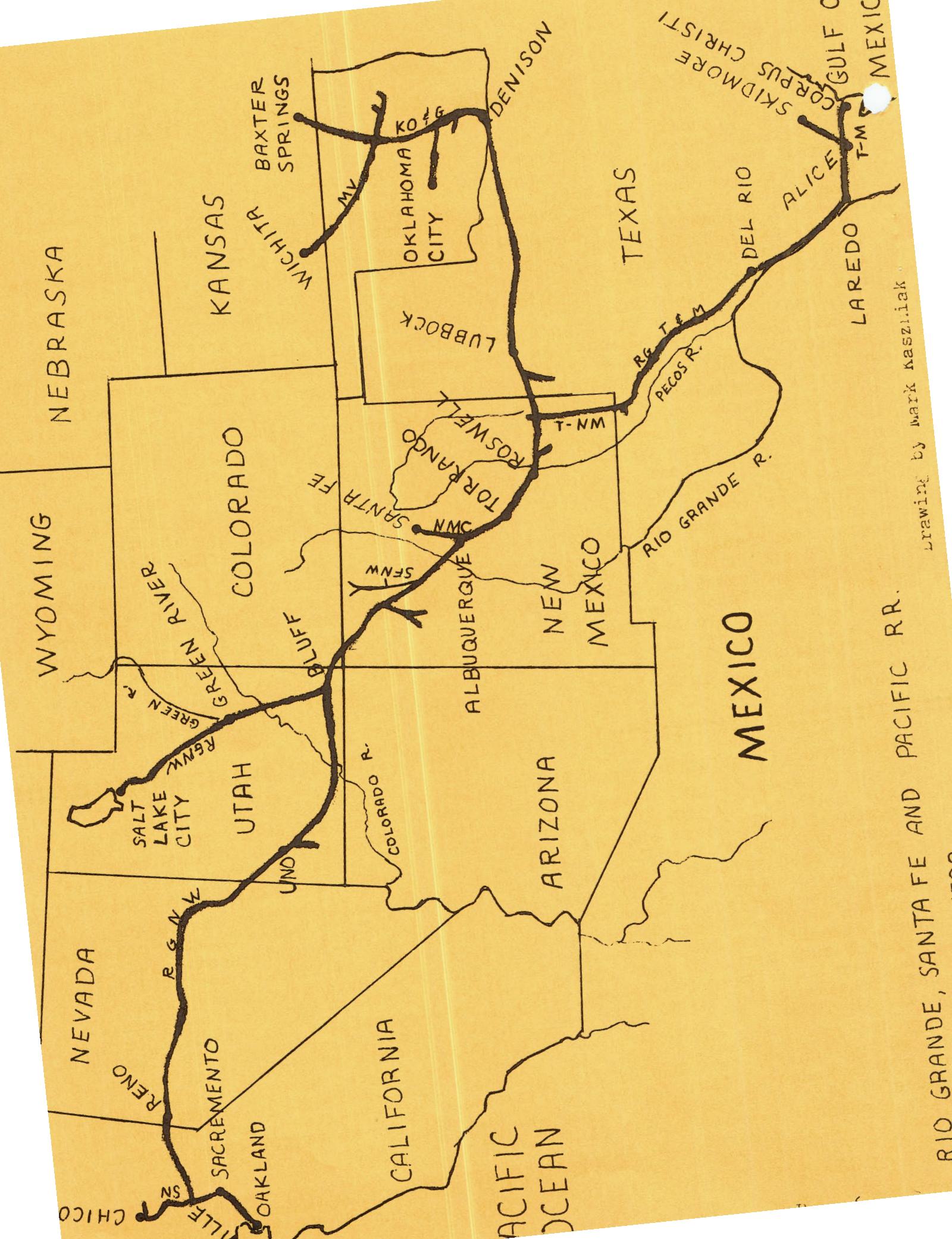
FOR SALE: Used HO GS4, great condition; 5 Athearn F7A shells; 1 GP18, powered; 1 0-6-0 steam engine; 1 Athearn PA1, powered; 1 GP20 shell; 1 AHM powered trolley; 2 SW1500's, no shell, but powered; 1 F7A custom painted BN, powered; 2 wide vision cabooses in BN; Athearn Passenger cars; Athearn tank cars, gondola cars and much more. So don't delay, send your SSAE toady for prices to: Jeff Scholler, 1747 Selby Ave., St. Paul, MN 55104

USED HO: Model Power: CP 2-8-2 \$28; CN E9 \$20; CN F9A (w kadee's) \$12.50; CP F9A (w kadee's) \$12; CN RS11 (w kadee's) \$15; RI Plymouth (w kadee's) \$12; Bachmann: Metroliner set \$40; Tyco: CP (gray & maroon, w. kadee's) F9a \$10; VIA Rail F9A \$9.50. LIMA: BCR C420 NEW \$25. Freight cars from

\$1.75 to \$6.00. Also buildings, accessories and track switches. All items shipped postpaid and insured. Write to: John Chambers, 2039 Ogilvie Road, Ottawa, Ontario, CANADA K1J 7P1.

USED HO SCALE: 1 Tyco Chattanooga Choo - Choo, 2-8-0 plus many boxcars--modern and a few old West. Have 20 brass snap-switches, some with switch machines. Have four Atlas twins. Write for what you want and prices. Trade considered for Athearn boxcars and/or diesel engines. Also I would like to talk to anyone that has built his/her layout using Tru-Scale self-gauging roadbed and has laid Hi-Speed switch kits. I need the do's and don'ts. Write to: James Morgan, 1117 E. Fishcreek, Grand Prairie, TX 75051.

GOT SOMETHING TO SELL OR TRADE? USE THE INTERCHANGE FOR FAST RESULTS.



Drawings by Mark Kassian

RIO GRANDE, JAMES

# MSC Report

Please address all comments and questions on this column to Dee Gilbert, MSC Chairman, Box 132, Harrison, AK 72601.

## THE INSIDE STORY

Your Member Services Committee (MSC) works out of a four foot by three and a half foot area reserved for the operation of the control panel of my L&D RR; it has ever since its beginnings. The idea of a Member Services Committee is not new to the TAMR, several versions have been around since the early 1970's. What makes this present version unique is the amount of organization that went in before hand.

For example, we developed a three part questionnaire which allows me to file everyone alphabetically by scale. We realize the space given on the questionnaire is limited, but currently the number one problem is the lack of information provided to some of the problems. To say your locomotive doesn't run, tells me nothing. To ask why your mountain collapsed and became a volcano is not much help either. I need specifics to be able to adequately help you. So feel free to use the back side of the sheet or attach additional pages to include more detail if necessary.

Now when I receive a questionnaire, the first thing I check is the scale you model. Next I study the question over to see if I personally can be of any help. Once this is done, I check other member's questionnaires to see who professes to have some knowledge of your particular problem. Then when I answer your request, I will always have my address listed first followed by yours--check to see if its correct. Next I answer what I can of your problem and refer you to others who may be able to help. However, it is up to you to write to these people for help and to advise me if they can't or won't help you.

Ocassionaly, I receive notes from beginners saying: "Sorry can't help now," "I don't know enough to help yet," or just leaving section three of the questionnaire blank. That's OK! Model railroading is a complex hobby. A beginner has too many

questions to be able to help out others. Still others are afraid they'll make a mistake helping someone and make that someone angry. You don't have to give help in the MSC in order to get it. That section of the questionnaire is there so as to make it easier for those who do want to help. So if you have a problem just jot it down and send it in, we'll try our best to solve it.

When you get a response to your problem from either myself or those you've been told to write, you may realize that it isn't the solution for which you've been searching. Study the answer anyway, perhaps a few alterations can make the idea work for you or provide a bit of inspiration needed to solve the problem by yourself. If you still are stumped, write me again and maybe another of our members will be able to help you out. I really care to see that you get the help you need. Thus I want you to tell me when you don't receive a satisfactory answer so that we can try again. There is no limit to the number of times you can write. So feel free to write again and again as new problems crop up.

Below is a list of areas where no information is presently available. If you feel that you can help in any of these areas, please feel free to write me.

Pennsy S2 - history and routes  
Those involved in Junior Achievement  
Prototype weighing of HO cars  
Actual passes of Frisco, MoPac and L&N  
O scale overhead, parts for construction and how to's.  
Use of Castolite for water, streams, rivers, lakes and waterfalls.  
Peco switch templates, full size for N scale--all switches.

Finally, I also welcome friendly letters on the subject of model railroading in general as this helps me gauge the average modeling talents of our members as well as to experience the ingenuity of our members. Then there is the added side benefit of coming across some new ideas that just might be suitable for publication in the HOTBOX. Already two good articles might be forthcoming as a result of such general correspondence with our members. So if the urge strikes, feel free to write.

## CHEYENNE & NORTHPORT

My railroad, the Cheyenne & Northport, is located in the Burlington Northern's Denver region which includes the Nebraska, Colorado, Alliance and Bug Tussel divisions. Consequently, the main commodity on my line is, of course, coal. Other major commodities handled by the railroad are: oil, lumber, live-stock, grain and ore pellets.

The origin of all my coal trains is Snake Belly, some simulated 75 scale miles east of Bug Tussel. Like the BN's Nebraska division, I plan on having a coal train leave Snake Belly every scale hour that my layout is in operation. These trains will serve the towns of Bug Tussel and Golpher Gultch, each of which has coal burning power plants. The power plants will have rotary car dumpers so the coal trains serving them will have to be equipped with rotary couplers.

My ore trains will originate 11 miles west of Bug Tussel and run straight through to Golpher Gultch. These trains will then head for points beyond eventually ending their run at the Great Lakes. These through trains will be designated Lake Ore Trains, but I have another ore train which serves the steel mill at Boardman. I call this train the Boardman Turn and it handles the ore pellets which are used for making steel.

I also plan to run several different freight trains, among them will be the BN's train number three; the Pacific Zip which is an east-bound TOFC transcontinental mail train. This will be one of my railroad's hottest trains and it will naturally make the management very angry if it is delayed or late into Golpher Gultch yard.

Furthermore, I plan to include both passenger and commuter trains on my layout. The commuter operations will consist of two scoots which will run four trips during each day's journey. Each of my two scoots (1 loco and 3 bi-level coaches) can carry a total of about 638 commuters. I chose bi-level coaches so that I could develop a

realistic push--pull operation. As for passenger trains, there are two Amtrak runs represented, the California Zephyr and the Midwestern Zephyr. However, my railroad also runs its own independent passenger service for the benefit of its local residents who have once again turned to rail travel due to the current energy problems our country is facing.

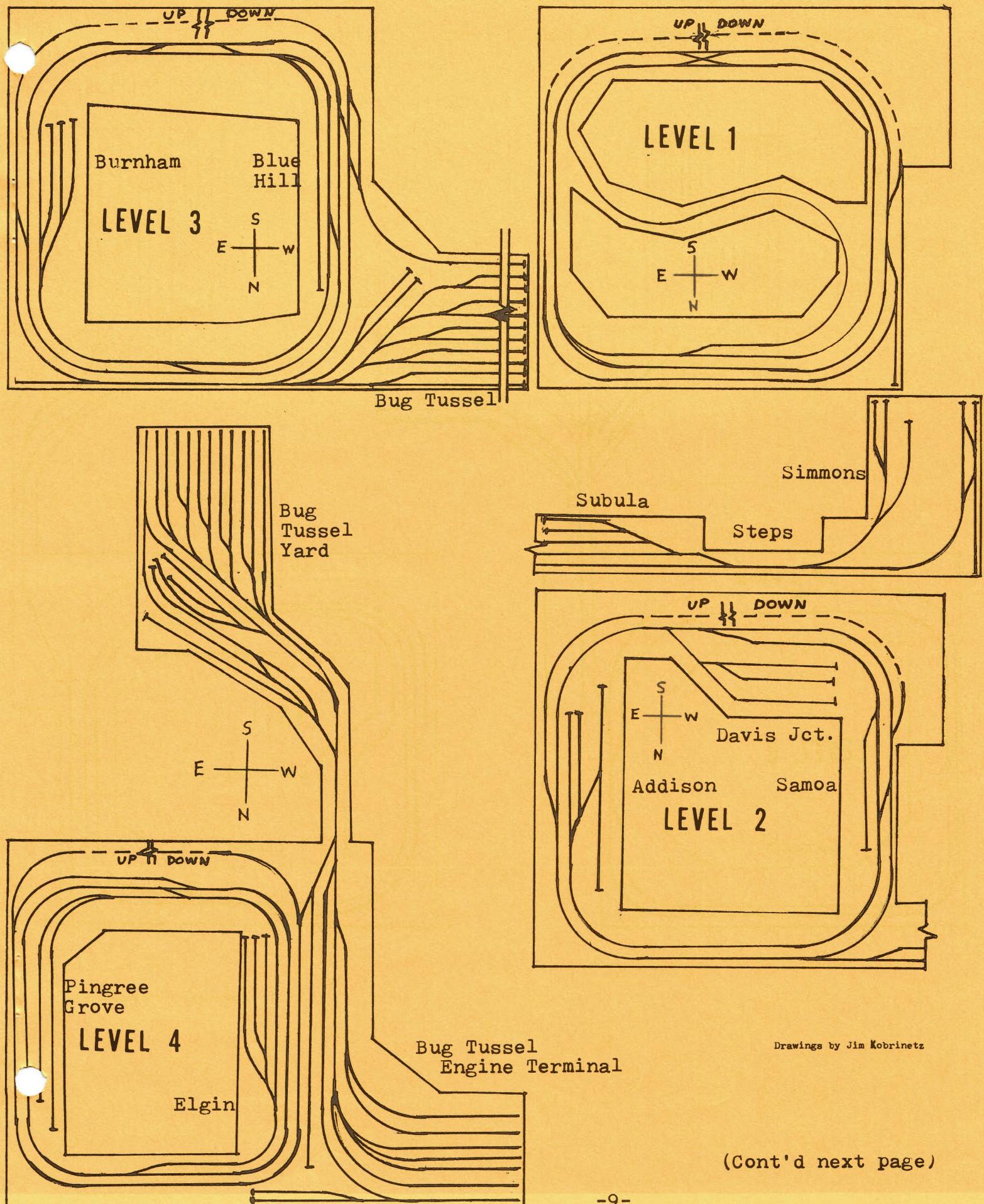
As you may have already guessed from the previous speculations as to the services my railroad plans to provide that it is currently in the construction phase. When it is completed, it will stand six feet, six inches tall have seven different wall shelves. The mainline will be single track with passing sidings and I plan to model some 15 towns on my layout which will surely justify the heavy traffic on my railroad.

Since I am attempting to get the feeling of going somewhere with my layout, I am equipping all my powered locomotives with a radio control system. This will mean that the engineer will have complete control over his train and it will also reduce a vast amount of very complicated block wiring. I also plan on making the entire layout CTC controlled by a dispatcher who will control all the electric turnouts and signals. I am also working on a two way talk system so that the dispatcher can communicate with tower operators and yardmasters.

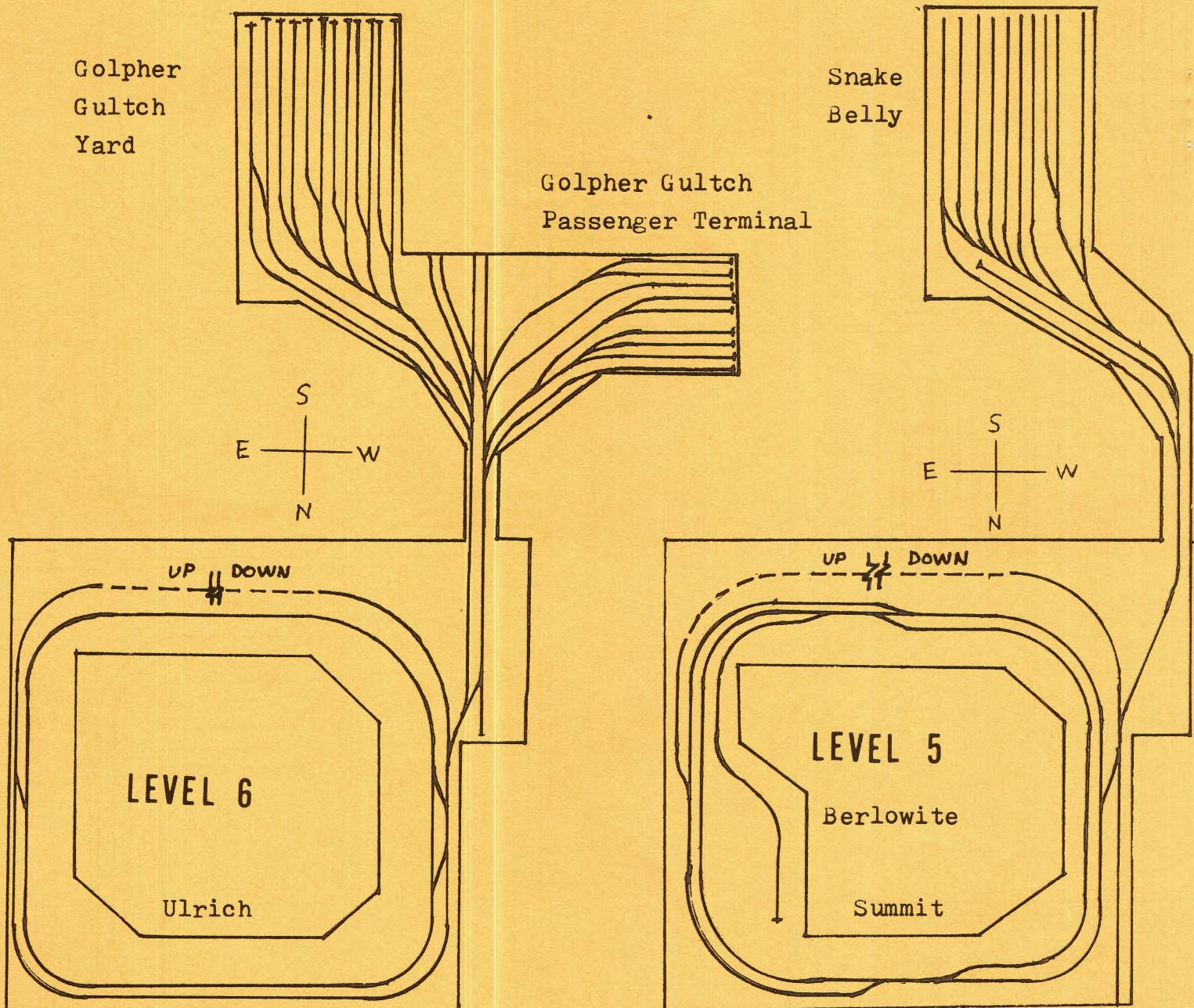
I decided to model my railroad like the Burlington Northern because my uncle has worked for the CB&Q and BN for the past 38 years. Currently, he is an engineer on the BN. To help me in my modeling, I have purchased a number of books on the BN and the roads that have merged into it.

Finally, I plan to run my model railroad as realistically as possible. I use situation cards, train orders, clearances and switching messages to add to the realism of the operating sessions which in turn helps to promote the feeling of really railroading a complete train.

CHEYENNE & NORTHPORT RR (Cont'd from last page)



CHEYENNE & NORTHPORT (Cont'd from last page)



Drawings by Jim Kobrinetz

By Dee Gilbert

# TIMBER

Surely your layout has at least one or two locations where bridges will be necessary in order to overcome some scale sized obstacle. Now there are many types of bridges that you could use, but one of the most impressive is the timber trestle. The sight of a steam locomotive pulling old wooden coaches over one of these structures means railroading to many modelers. Yet while timber trestles look impressive, a lot of hard work is required to make them so. However, when you've completed one or two, I think you'll agree that the time was well spent.

Let me start out by saying that I don't consider my trestles really timber, metal or specific copies of the prototype. I consider my trestles merely routings over voids designed to make the viewer convince himself that he is seeing a trestle. I have obviously accomplished this goal, as those who have seen the trestles on my pike rave about their appearance.

I often find that many people, myself included, approach trestle building in too much of a hurry to do a good job. When I first started out, I was after something cheap, neat and quick and the kits that I investigated were BLAA! In my first attempt, I used framing pieces that were way too large and nearly gagged on the results. Then I discussed my problem with my hobby shop dealer and he explained what I was doing wrong. I was going WAY out of scale. So I immediately purchased a bunch of his smallest framing sticks (1/16" and 1/8" for my N scale trestles) and found my end results very pleasing.

My trestles require the simplest of tools and materials. The tools you will need are: a pair of long needlenose pliers, tweezers, a scale rule and a very sharp knife (X-Acto is fine) or a razor saw. The building materials are various sizes of balsa wood. You will notice that I said balsa wood, not bass wood. I use

# TRESTLES

balsa because it is easy to work with. Bass wood is much stronger, but isn't as flexible. Besides, when all the bracing is finished, you'd be surprised at the amount of strength that balsa has--certainly enough to carry your heaviest train.

You begin by measuring out a piece of track to span the area where your trestle is to be built. I suggest that you use a single piece of flex-track for this purpose. This way, you won't have to worry about the sectional track pieces coming apart while you're building your trestle. You can use sectional track if you want to, but you should then solder the pieces together to form one continuous section. This will not only make your trackwork on the trestle one rigid piece, but will also improve electrical continuity in an area where you really don't want any problems.

Once the track is ready, you can glue the largest of your balsa pieces (1/8" in N scale) to the bottom of the trackties. Spread the glue along the ties and press the balsa in place. Be sure to use a glue that is compatible with the two components that you are gluing. If you are building a curved trestle, you will have to carefully curve the balsa into place. You can use clamps or other devices to hold the wood while the glue is drying, but be sure that the glue is fully dry before attaching the supports.

The next items to be constructed are the main supports (fig. 2) that hold the trestle up. These are best constructed on your workbench and then installed directly on your pike. Of course, the height of your supports will depend on the distance from the track ties to your benchwork. Once you have determined this height, the main vertical supports can be cut. Then the horizontal cross pieces can be cut and glued to the vertical pieces at approximately 20 scale foot intervals. Your vertical supports can be either vertical or on a slight

(Cont'd next page)

## Timber Trestles (Cont'd from last page)

angle. Angling the supports is much more prototypical and helps to distribute the weight of your models over a wider base area. A typical slope angle for the vertical supports is 12 to 1, or stated another way, 12 feet of rise for every one foot the vertical piece is angled.

After the horizontal pieces have been glued to the vertical pieces, the cross bracing can be applied. If your bridge requires many of these supports--I place them at 10 scale foot intervals--you may want to construct a jig. The jig, made out of a piece of scrap wood and small nails, will hold the bracing pieces in correct alignment while the glue is drying. If you place a piece of wax paper over the piece of wood, your supports will not become glued to the jig. Of course, once you've made the first support, you will know the proper length to cut each piece of wood and therefore the remaining supports will go together much faster.

When your supports are completed, you can mount them to your benchwork. It's easier to do this before you've installed scenery, but various supports can be cut and the cross bracing reworked if your scenery is already in place. After the supports are in place, the track with the horizontal beams attached can be glued to the supports. Now you can run a few trains over your bridge to determine if it sags. If it does, then more supports will be needed. If not, the horizontal bracing that connects the supports can be glued on. This is usually the most frustrating part of the project because you are now working on the bridge while it is on the layout. This is where your long needlenose pliers and tweezers come in handy. With only ten scale feet between the supports, you'll need something smaller than your hands to affix those braces!

Now that the horizontal braces have been applied, your trestle is about 99% completed. All you have to do now is stain it (if you want to, I didn't as balsa ages over the years) and add other necessary details such as fire barrels and platforms so your

scale railroad workers don't get caught on the trestle with no recourse if a train approaches. Other items that can be added is lichen around bottom of the supports and telephone wires strung across one side. Adding all these details makes the trestle look more impressive and helps to hide the fact that it wasn't built entirely to scale.

### ATTENTION!!!

#### ALL MID-WEST RAILFANS

If you're really into mid-western railroading and railfanning, then you will be interested in the Central Region's WAYFREIGHT (WF). As of January '81, the WF will be produced as a monthly publication with the new Railfan's Guide to the Mid-west; including the regulars such as the Rail Notes column that keeps you up to date as to what's happening in Great Mid-west railroading; the Rip Track, conducted by Jeff Wilke, containing modeling projects of interest to all; and current info on the MESS, provided by Mark Kaszniak. All this and more, in the Central Region WAYFREIGHT!!!

To join or for further information, contact:

Corby Anderson  
Central Region Representative  
1209-97th Avenue  
Kenosha, Wisconsin 53142



JOIN THE CENTRAL REGION TODAY

# TIMBER TRESTLES (Cont'd from last page)

Figure 3:  
Close Up  
of cross  
bracing.

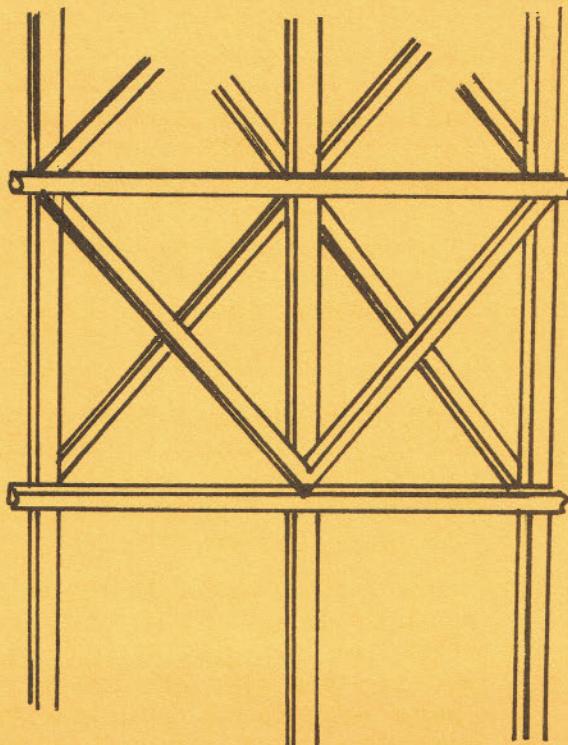
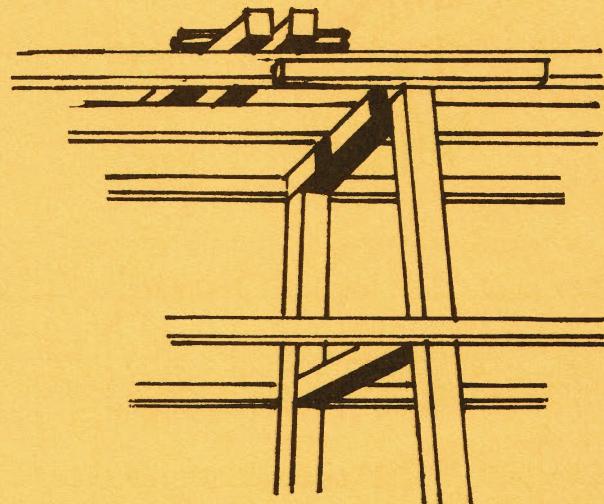


Figure 1:



Basic Main Supports shown  
in perspective view.

Cross Bracing can be  $1/16"$  or  $1/32"$  square pieces in N scale. Vertical and horizontal support pieces are  $1/16"$  square.

Figure 2: Vertical View

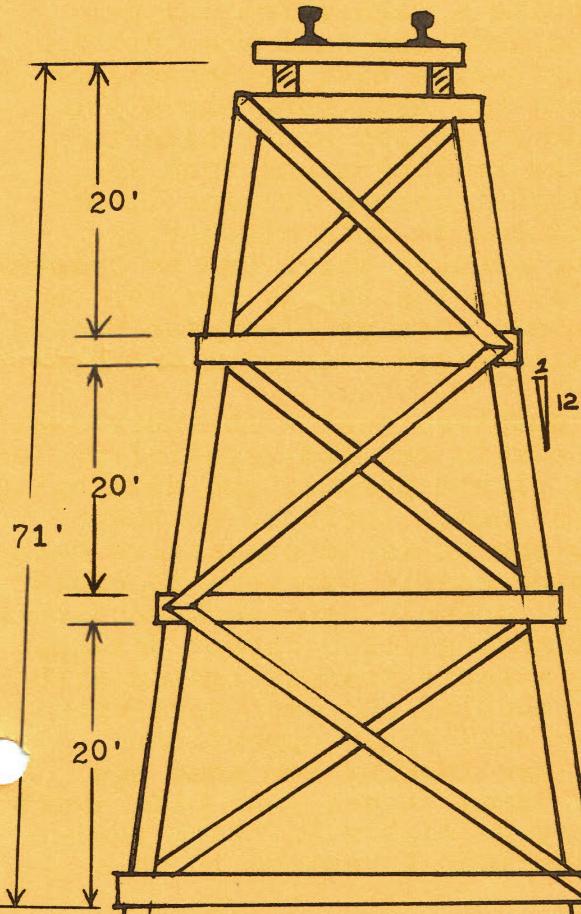
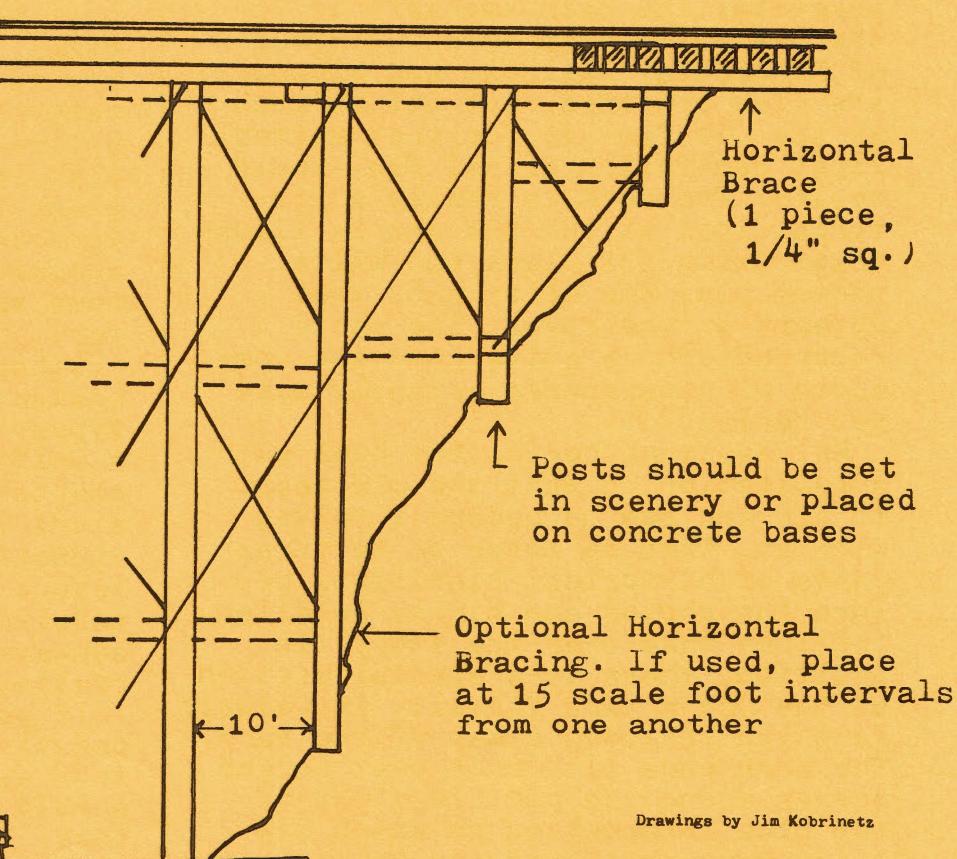
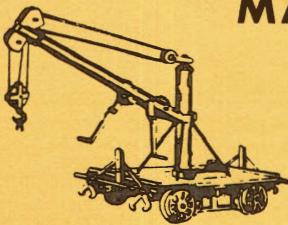


Figure 4: Side View



Drawings by Jim Kobilnetz



# MAINTENANCE OF WAY

M of W is a product review column written by our members on model railroading and railfanning items that may be of interest to you. All the opinions presented are those of the reviewer and are not necessarily those of the T&W or the HOTBOX. Please submit reviews to the HOTBOX Editor.

Chicago Great Western--Iowa in the Merger Decade by Phillip R. Hastings. Carstens Publications, P.O. Box 700, Newton, NJ 07860. \$9.95 postpaid. 80 pages. Softcover.

Carstens has been coming out with a rash of new prototype softcover books and the latest in this line is about the Chicago Great Western Railway. The CGW was a 1,500 mile railroad that began in 1888 and subsequently ended with a merger into the C&NW in 1968. CGW fans, like myself, have long awaited a book of this type. However, as the title suggests, the main emphasis is on the CGW's lines in Iowa. Glimpses of Minnesota, Illinois and Missouri are also provided, but not as much as any CGW fan--or for that matter, first generation diesel fan--would desire. The book is really a pictorial look at the CGW and is illustrated with 132 black and white photos plus one color shot that appears on the cover. The photo reproduction is quite good and some shots of snow scenes actually make you feel cold!

Mr. Hastings begins the book with a reflection on what the CGW meant to him and how he became involved with it. Then he moves on to three pages of historical highlights before the photos and highly detailed captions take over the story. In addition, there is a special section on the CGW's yard facility in Oelwein IA which includes a map of the yard. The book ends with some post merger scenes along the old CGW right-of-way plus information on which CGW lines were abandoned and those that

are still used by the C&NW.

For any diesel fan who loves to see eight unit lashups of "chicken wire" F3's, F7's or Alco RS units plus early EMD hood units, I highly recommend this book. I know that it is a great addition to my collection (as it also contains an all time diesel roster and system map) and I'm sure it will be to all other fans of this midwestern road.

--Gerry Dobey

Creative Layout Design by John Armstrong. Kalmbach Publishing, 1027 N. Seventh Street, Milwaukee, WI 53233. \$11.95 plus 75¢ postage and handling. 125 pages. Softcover.

John Armstrong is back and he's brought his bag of designing tricks with him. Creative Layout Design takes you beyond the limits of your available space and shows you how to go about creating that one-of-a-kind layout that you've dreamed about.

Each chapter takes on a different design concept and shows you how to utilize its advantages and potential disadvantages effectively. For example, the first chapter deals with pike locations. Mr. Armstrong shows you how to build a pike which can be folded up into the wall when not in use. Many people have designed such pikes, but Mr. Armstrong shows you how to do it so you won't have to remove the rolling stock from the entire layout before folding it up! Also included in this chapter are plans for garages which can be raised vertically so the family car can be accommodated, special plans for mobile home trailers and even your own backyard.

Other chapters deal with individual scales, scenic realism, special prototypes, a pike's purpose and developing a unified theme. Included in these chapters are hints on suggestive omission; using scenic barriers to create vignettes; movable benchwork sections; layout lighting; club layout design; modeling a rapid transit system with subways; modules and even developing realistic unit train operation.

If you are looking for some new creative ideas to set your pike apart from everyone else's, I think you should consider investing in this book.

--MAK

# TRAIN ORDERS



TRAIN ORDERS is a letters column in the TAMR HOTBOX where you can express your views on the TAMR, its publications and its officers. All letters for this column should be sent to the Editor of the TAMR HOTBOX.

I wasn't sure if I should try the TAMR for another year as there isn't much to the club. The HOTBOX isn't too bad so I'll stick it out another year. However, if things don't get better this will be my last year. I hope the TAMR will be a little different this year. --Remi Dalcourt  
Wahnapitae, Ont.

Your organization sounds promising!

--Steve Harris  
Calgary, Alberta

As you may have guessed, I have decided not to renew with the TAMR. One thing I don't really understand is why you decided to go strictly modeling--wouldn't it be fair to let all members vote on the subject? Maybe more members would favor to go strictly modeling, but I still think it would have been more fair to vote on such a drastic change as this.

--Pat Seward  
Bay City, MI

(Editor's Note: Almost everyone knows there is plenty of railfanning material in the TAMR. Most of our regions cater to the railfan and even the HOTBOX plans to still print prototype subjects from time to time)

I've given a lot of thought about renewing. I am 75 years young (?); have dismantled all my model railroad; gave all my HO to my son and grandson; my eyesight has gone downward causing hard-to-read small type. My many years with model railroading have been interesting, but now I must slow down. The TAMR publications have been interesting. Best wishes and thanks.

--Ted Zehrung  
Eugene, OR

(E.D: Sorry to see you go, Ted!)

I like the new HOTBOX. You guys are doing a terrific job back there. Very, very professional. Good work.

--Gary Gardner  
Salt Lake City

I just received my July-August issue of the HOTBOX and with it was one of your TAMR renewal notices. Thank you. I was not quite sure on when and how I was supposed to renew. I think the TAMR is really worth the money. So worth it, in fact, that I'm probably going to renew on a sustaining membership. Keep up the good work!

--Eric Hagman  
Dover, NH

(E.D: For all others interested, the number on your membership card corresponds to the issue number of the HOTBOX which will contain your renewal notice.)

I have not been active in the TAMR until now. I received the July/August issue and had to write. It is the best one ever! The Colorado Midland, Wyo Central, Helpers, M of W were all great. I liked the Card Order Operation article and am going to start one soon. My pike isn't much (4x8'), but I have a number of ideas for improving it thanks to the TAMR and the HOTBOX. I guess you know by now I'm totally satisfied and definitely renewing my membership.

--Chris Neyen  
Pearl River, NY

I am writing in response to the several letters in HOTBOX #162 which were opposed to running "railfan stories." I am a college student and member of the Penn State Model Railroad Club which presently has about 30 members. Many of those members never go railfanning and consequently problems arise. Such things as 1890 Reno's pulling high cubes with a Conrail caboose, or an open hopper supposedly hauling processed white sugar are commonly seen. I am not a purist, but I try to make my railroad as realistic as possible. Railfanning, or at least some intelligent observation, is an integral part of model railroading. I think you should continue to print prototype material, while continuing to put more emphasis on modeling.

--David Williamson  
Univ. Park, PA

**FRONT COVER:** Bridges are an important scenic item on any layout as our cover photo for this issue illustrates and thus patience and care must be observed in their construction. On page 11, Dee Gilbert will show you how easy, but time consuming, it is to construct eye-catching timber trestles. Photo by Gerry Dobey.

## **MARKERS:**

**IN THE NEXT ISSUE:** Our Auditor, Greg Dahl, shows you how to plan your freight car fleet. Kevin Garrett takes us on a tour of his Golden RR. Guy Paterson and the HOTBOX Staff show how to incorporate a wye in your layout using a Canadian National prototype at Brown Point and the Editor will be playing Santa Claus again and handing out some gifts to various officials and members that are not too badly needed. All this plus our regular columns--**HELPERS**, **TRAIN ORDERS**, **M OF W**--will be coming your way in the Christmas issue of the Un-Magazine of Model Railroading.

**1980 NATIONAL CONVENTION:** How was it? Who attended? We plan to have a report for you soon. However, it isn't too early to start thinking about where next year's convention should be held. Our President, Jeff Wilke (38115 Park Street, Oconomowoc, WI 53066), will be distributing bid forms this coming January. Write him if you wish to be placed on the mailing list.

**TAMR HOTBOX**, "the Un-Magazine of Model Railroading"  
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