

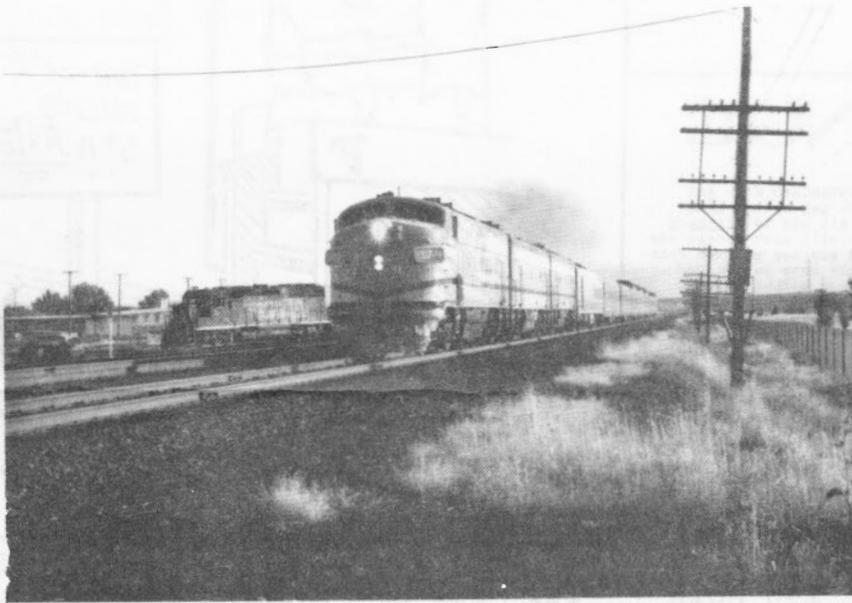


HOTBOX

"the Un-Magazine of Model Railroading"

No. 160

March - April 1980



A

Tribute

to

the

RIO

GRANDE

ZEPHYR



HOTBOX

Critters: Scott Sackett

OFFICIAL PUBLICATION * Train Association of Model Railroading

Issued every other month with an additional special mailing of a Directory of Membership during the summer.

Annual dues for the TAMR are as follows:

- REGULAR (under 21 years of age): \$5.00
- ASSOCIATE (21 years of age and up): \$4.50
- MAINTAINING (both Regular & Associate): \$10.00

Please address all membership applications, renewals, address changes and complaints of non-receipt of the TAMR HOTBOX to the TAMR Secretary.

Gerry Dobey, TAMR Secretary
 145 E. Kenilworth Ave.
 Villa Park, IL 60181

All other HOTBOX business, except where specifically noted, is handled by the Editor. DEADLINE--All material for publication must be submitted for consideration by the first day of the first month of the issue (i.e. for May/June issue, material must be received no later than May 1st). The TAMR HOTBOX assumes that all material is contributed gratis and no payment will be made upon publication.

Mark Kaszniak, HOTBOX Editor
 4818 W. George Street
 Chicago, IL 60641

INTERCHANGE

If you have something to Buy, Sell or Trade, use the INTERCHANGE to get results. Your ad is seen by all TAMR members. Rate: 10¢ per column line (35 spaces), your name and address printed FREE. Send all ads to: Richard Sonoski, 219 First Street, Port Carbon, PA 17965. Rich will also accept ads for your pike too. Rate: 20¢ per column line, name and address printed FREE. If you want your herald or other artwork reproduced, the rate is 50¢ per square column inch. Send in those ads today and make your HOTBOX better tomorrow.

N - Scale Used Listing: CN GP30, \$10 PRR FP45, \$14.50, both by Lima. Other locomotives, freight cars (\$1.5 - 2), buildings (\$1.5 & up) and track available.

HO Scale Used Listing: 0-4-0 Shifter and tender with Kadee's front and back, \$15. 2-6-2 Santa Fe Prairie, \$20 both by Tyco. Other equipment also available. All items post paid and insured. Trade considered. Specify list.

Please write to: John Chambers, 2039 Ogilvie Road, Ottawa, Ontario, CANADA K1J 7P1.

Lone Eagle Payne informs us that the calendars he was offering in the Nov-Dec 1979 HOTBOX are all gone. So don't write and ask for one!

Wanted: Lionel #300 Hellgate bridge, #408E engine in Mojave plus any other Standard or 'O' gauge trains and accessories from 1910 - 1960. Will pay top dollar or Gold. Contact: Anthony Casamassima, 62-48 84 Street, Middle Village, NY 11379.



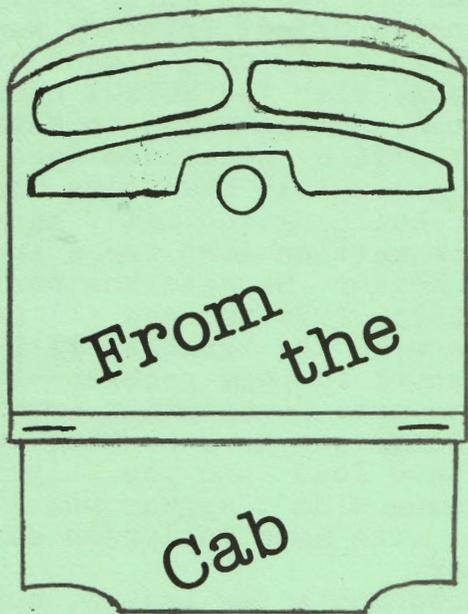
WANTED: Any or all items relating to the C&NW RR. Looking for slides, pictures, plans, maps, books, etc. Please state condition of items and numbers of units on slides and pictures. Also location and prices. Contact: Gerry Dobey, 145 E. Kenilworth Ave., Villa Park, IL 60181.

TRAIN ORDERS

TRAIN ORDERS is a letters column in the TAMR HOTBOX where you can express your views on the TAMR, its publications and its officers. All letters for this column should be sent to the Editor of the TAMR HOTBOX.

Why do all the layouts featured in the last few issues all have a town named "Kobrinetz" on them?

(Editor's Note: The name "Kobrinetz" is not a town name, but that of our Graphics Editor, Jim Kobrinetz. We are instituting a new policy to avoid this source of confusion in the future.)



FINAL FAREWELL?

I wonder if it will stay this time? Seems every couple of years I retire, for good, from this or that and before too long someone decides I'd better work somewhere.

The thrills of actually getting to take pictures while railfanning got too great and I have resigned again. Mark Kaszniak, who has done all the layout work on the last couple of issues, is taking over (You fool!). I wonder what they'll find for me to do now?

If there's not a whole lot to say, you're right. I'll refer you back to one of my previous farewell addresses, from this or other posts. Yet I will be nasty and get after a few people since it doesn't matter now.

To wit: It is high time that more than one person in this association quits playing elementary (or kindergarten) children and runs this group like responsible people. I know that you'll run into people in this association that you don't like and that you'll always disagree with someone. I hate to break the bubble, boys, but this is the way life is. If we spent as much time promoting this association as we do bickering about the size of periods and commas, where could we be? The thrill of railfanning is in the pictures and sights, not trying to dump someone else's car being the first to nowhere.

I don't understand why our hobbies of modeling and railfanning seem so bent on self-destruction. Modelers bicker about plastic, brass, couplers and all that rather than just having fun. Railfans join motorcades that belong on racetracks.

I think I've said enough, If you think the above doesn't apply to you, think again. If it does, find another area that applies. We're not a bunch of Iranians or whatever else, are we?

Peace be with you all.

OVER A DECADE OF SERVICE

Tim Vermande and I have known each other for about five years and over that time we have become good friends. We are frequent railfan companions and have often had intense arguments over the direction of this publication as well as the TAMR. Now I know that Tim will probably not like what I'm about to do, being the modest man that he is, but I'm going to do it anyway because I believe that our membership should know the amount of time and hard work Tim has given to the TAMR. However, to properly proceed, we must begin at the beginning....

Sometime in 1968, Timothy J. Vermande joined the TAMR; he was 14 years old. In November of 1970, Tim became an active member of the HOTBOX Staff by succeeding Richard Jahn as Circulation Manager. These were the so called glory days of the TAMR. Tom Papadeas, then HOTBOX Editor, was in his prime as was Dick Wagie, our Publisher at the time. So what if the HOTBOX came out a little late, the average issue was offset printed and had a total of twenty pages. Membership in the association was constantly increasing and the future looked bright.

By January of 1971, Tim (in addition to Circulation Manager) was both Editor and Publisher of the Great Lakes Region (now Central) WAYFREIGHT. In the March/April 1971 edition of the HOTBOX, Publisher Dick Wagie wrote an article which explained in detail how the HOTBOX was produced and asked for help with the various production processes. Also in that issue, Terry Burke replaced Tim as Publisher of the GLR WAYFREIGHT.

(cont'd next page)

Tim responded to Dick's plea and by the July/August issue, he was made HOTBOX Typist at which time he relinquished his duties as Circulation Manager. In addition, Tim also decided to run for the office of Vice President (now Auditor) in the TAMR. Naturally, he won.

During his term as Vice President, Tim remained the HOTBOX's typist. While not much is known about his administration, as he was not one to "brag" about his accomplishments in the HOTBOX, it is known that our membership increased from 155 to about 240.

In late 1972, Tim was renominated for Vice President (at the time, all elected officers held single year terms), but he declined. Why? Well, the situation is a little bit complicated, but I'll try to explain: Just after Tim was elected Vice President, Tom Papadeas resigned as HOTBOX Editor and Mike Bonk replaced him in March of 1972. Around Christmas, just after elections, Dick Wagie resigned as Publisher and Mike Bonk either resigned or quit as Editor. In addition, there were various problems in the Executive Board and growing apathy among the membership. Tim thought he might succeed Mike Bonk as Editor, but John Held replaced him. Tim then went back to being an ordinary member, for the time being.

In 1973/74, John Held tried to get the HOTBOX back on schedule, but he just couldn't do it. First, there was trouble in finding a printer. Then there was trouble with the printer. Several issues were lost and those that did come out were constantly late. While all this was going on, the TAMR was still gaining members and in 1974 our membership stood at an all time high of 333; however, few had received their money's worth in HOTBOXs or benefits. The officers then decided to ask all members to renew due to the loss of the official records plus a money shortage from expensive offset HOTBOXs. Needless to say, the effects were disastrous: TAMR membership plummeted and in November of 1974, John Held resigned as HOTBOX Editor.

Dale Madison, acting President after the resignation of Bengt Muten, appointed Tim editor when he volun-

teered for the job in March of 1975. The expensive offset printing was put aside in favor of a mimeo, on time HOTBOX. With the HOTBOX back on schedule, things began to settle down in the TAMR. Elections were held and a complete Executive Board took to rebuilding the TAMR. Meanwhile, Tim settled down for a long stint as Editor. He held the position through 1975, 1976 and was all set to retire with the March/April 1977 issue, except for one problem--it never came out. Circulation problems were to blame and as a result, two HOTBOXs were lost and the July/August issue didn't appear until September with Mark Tomlonson as the new Editor.

Mark produced a few issues, but then he moved and stopped communicating with the staff and officers. Thus in March of 1979, two years after his departure, Tim was once again asked to resume his old position. He did and has remained until now. Tim has been responsible for the production of twenty HOTBOXs, 19 of which came out on time. I think this is a pretty impressive record and one which all TAMR members should be proud of.

Furthermore, Tim has written numerous articles on photography for these pages and many of his photos have graced the cover of this as well as our regional newsletters. Such total dedication to an association (free of charge) is rare and should be commemorated. I wish that I could wish Tim a happy retirement from his toils in the TAMR, but he isn't through yet. He still remains the TAMR Archivist who keeps all our past records in a safe place and he will undoubtedly be named to some other "official" position in the TAMR in the future. Also, it is more than likely his photos and articles will be appearing in these pages for some time to come. I say "hats off" to Tim Vermande for services already, and in the future to be, rendered.

The Member Services Committee can help you with your problems, both mo and prototype. Help is just a letter away, so write: Dee Gilbert, MSC Chairman, Box 132, Harrison, AR 72601

Riding the



Zephyr

Upon arriving at the D&RGW depot in Salt Lake City, the first thing we see is the consist hissing quietly in the early morning dusk. Three aging F units and an old Alco PB lead our silver streamliner followed by ten stainless steel cars capped by a round-end observation with wagging red tail light. We board the Silver Lady and pick a seat in the Silver Pony, an ex-California Zephyr coach. After stowing our luggage in an overhead rack, we head for the diner to sample some Rio Grande cuisine.

The dining car steward, Mil Lundquist, seats us at a well-set table, complete with CZ tablecloth and informs us that breakfast will be served as soon as the train departs. In the meantime, we look out the window, watch the last "good-byes" and listen for the "All Aboard!" The train gives a slight tug and we are underway, right on time--7:00 AM--beginning our 570 mile, all day, journey to the mile high city of Denver.

Gliding through the back streets of Salt Lake City, with wonderful smells emitting from the kitchen, we watch the sun break over the Wasatch mountains. Suddenly the valley is flooded with light making the stainless steel on the Silver Lady glisten.

Breakfast is served in a grand manner with hot plates and starch waiters. Five huge hunks of French toast, lightly coated with powdered sugar and thick maple syrup; two sausage patties and orange juice make up the Rio Grande's breakfast special for only \$2.45! While eating, we roar through Murray, Utah at 70 mph. passing many auto wreacking yards and reminding me of a line from Steve Goodman's City of New Orleans: "passing graveyards of the rusted automobiles, Good Morning America!"

Provo is our first stop, a mere 44 miles from Salt Lake City transversed in fifty minutes. Leaving Provo at

7:50, we pass the well kept yard of the Utah Railway, one of the last strongholds of Alco power, and within moments enter Spanish Fork canyon beginning our first mountain ascent of the day.

In the canyon, the Rio Grande Zephyr twists and winds following the river up the red canyon walls. Towards the top of the canyon, the tracks take four switchbacks adding ten miles to the trip without getting any closer to Denver. After cresting at Solider Summit, we drift down the canyon of the Price river. Past UP&L's power plant, Utah Rwy. yards, through Castle Gate and into Helper, UT. Helper is one of the few helper stations left in the United States.

The Silver Lady pauses here for a crew change, it's now 10:00 AM. Leaving Helper, we pass through Price and head into the Utah desert. Time for a game of cards and a Coke in the lounge. In the summer, this is the time to ride in the vestibule, but now in the winter it is much too cold. At 11:15, we pause briefly at Green River, UT and then cross the mighty Green river flowing south to meet the Colorado--this is the lowest point on the entire Rio Grande system. Twenty minutes later, we stop at Thompson, a small mining town, but quickly move onwards.

Soon we enter Ruby canyon where we join the Colorado river and leave Utah. Our silver streamliner carefully winds among the pink cliffs which are so close that you can practically reach out and touch them. We follow the Colorado river all the way to the Continental Divide and watch it shrink into a small creek, much smaller than the half mile wide river it was when we joined it. Again, we head to the diner, this time for lunch--hot roast beef sandwiches and apple pie.

Grand Junction is our second crew

(cont'd next page)

change stop and we will take on water here as well. Thus we have fifteen minutes to stretch our legs and take a few photos of the train. We leave Grand Junction promptly at 1:25 PM and begin passing many fruit orchards all at 70 mph. However, soon we slow down as we enter the canyon of the Colorado river. We wind and twist while watching the F units work their tails off as they are constantly in view pouring out thick black smoke while chanting loudly.

At 2:35 PM on the dot, we pull into a small Colorado town named Rifle and again quickly depart. Still we are climbing upward through the canyon of the Colorado river. Thirty-five minutes later, the Rio Grande Zephyr pulls into a small ski town named Glenwood Springs, near Aspen. This is where many skiers board in the winter months for the short trip to Denver.

We depart Glenwood Springs and begin our ascent of the breathtaking Glenwood canyon. Sheer rock walls rise hundreds of feet on either side of the narrow river channel with the tracks perched on the edge of the water. We pass Grizley siding where the California Zephyr's met and continue our climb up Glenwood canyon.

At Dotsero, the Rio Grande's main-line splits off and heads down the Royal Gorge, we diverge onto the Dotsero cutoff and head east to Bond. At Bond, we join the old Denver & Salt Lake RR line and head towards Denver. On we go through the mighty Rocky Mountains and at 6:20 PM, a short stop is made at Granby. This is the last stop before Denver.

As we enter the Winter Park region and curve through Tabernash, suddenly we are plunged into darkness. We have entered Moffat tunnel; 6.1 miles of blackness lasting for over ten minutes. Popping into daylight once again, we now head down hill. Now it's time for dinner and the Rio Grande's specialty: broiled Rocky Mountain trout, potatoes, rolls and a sundae for dessert. Eating has to be one of the best parts of the trips. How such good food can come from a steamy, hot steel closet of a kitchen is a miracle.

Through South Boulder Creek canyon and some thirty odd tunnels we pass
(cont'd next column)

RGZ Roster

Locomotives:

#5571 - F9A	#5762 - F9B
#5763 - F9B	#301 - Steam Gen. (ex-Alco PB unit)

Coach - Baggage:

#1230
#1231 (ex-pro prospector)

Flattop Coach:

#1121 Silver Pine
1120 Silver Aspen
(ex-CZ section sleeper; converted 1964)

Dome Coach:

#1108 Silver Pony #1106 Silver Colt
#1105 Silver Bronco #1107 Sil. Mustang
(ez-CZ dome coaches)

Food Service Cars:

#1115 Silver Bangquet (ex-CZ diner)
#1140 Silver Shop
(ex CZ dome-dorm-coffee shop)

Dome Lounge Observation:

#1145 Silver Sky (ex-CZ dome-lounge
sleeper, sleeper rooms removed 1975)

Typical Consist:

F9 A-B-B (summer)
F9 A-B- Steam Generator B (winter)

Baggage Coach

Dome Coach

Dome Coach

Dome Coach*

Flattop Coach*

Dome Coach*

Flattop Coach*

Diner

Dome Lounge Observation

* Put on as needed for large crowds;
summer, Christmas, etc.

as we approach Denver. From high in the mountains, we can see the city lights far below. Soon, we are winding down the front range, around Big 10 curve and into the outskirts of Denver. We gather our things and prepare to leave our home for the past 14 hours. The gental stop of the train indicates that we have just reached our destination--Denver Union Station. We check our watch, 9:00 PM on the dot. We almost hate to leave, but we do because we know we'll be back--someday.

TAMR HOTBOX

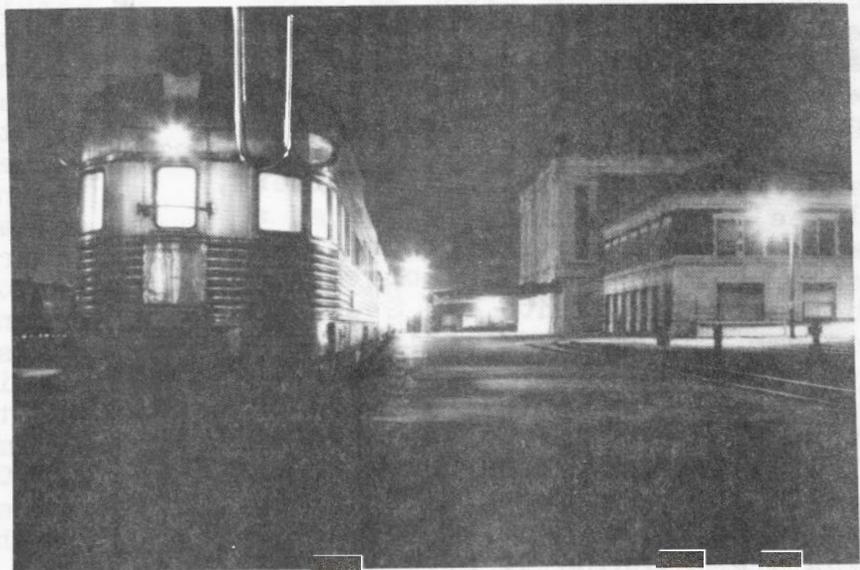


(TOP) The Zephyr ready to depart Salt Lake City station

(Middle) Tail end of the Zephyr as it rests during the evening in Salt Lake City

(Bottom) On its tri-weekly run passing Grant Tower Road.

All photos: Gary R. Gardner



From Our President's Pen

I don't know about you, but I personally feel the Student Fare column in MODEL RAILROADER magazine has declined in the last year and a half. That was also the general feeling at our mid-winter Executive Board meeting that was held in Chicago and attended by Mark Kaszniak, Gerry Dobey and myself. In order that our opinions be heard on this subject, we composed a letter stating that we felt that Student Fare should contain articles about student's modeling techniques and layouts rather than turning the column into a monthly gossip section. We sent our letter to Russ Larson, Editor of MODEL RAILROADER, and received a reply that stated in part: "if you can find us teenagers who have layouts that our photogenic, who are capable of taking close-up black and white photos and who can write an article about their layout, then we (the TAMR) should refer that person to MODEL RAILROADER."

Being the largest model railroading association exclusively for teens, I feel that the association as a whole should back an effort to get some quality stories about our members modelwork into the Student Fare column. So, to start this off, if you do really nice model work, but are unable to write an article or take clear photos, please contact me. I will then make the necessary arrangements to get everything together and to MODEL RAILROADER. Thus if any of you are planning to submit an article to Student Fare (and some of you have already), please write and tell me about it so that I can inform Russ Larsen. After all, if your article is published, it is a reflection on the whole teen modeling population and it is our duty to present an accurate picture of what teens are accomplishing in model railroading.

Finally, on a totally unrelated matter: Upon looking through the 1979 TAMR Directory, I counted 104 members who operate HO scale layouts. So far, 23 TAMR boxcars have been sold to 18 different members. I think that more of you could help out the TAMR by buying one of these cars. They are standard Athearn 40' boxcars painted

boxcar red and lettered with custom made decals from Herald King. We have a total of 75 cars available and since response has been far below what was anticipated, we probably won't be ordering any additional ones after these are gone so this is truly a limited run set, sure to be collector's items. So the next time you buy a car for your road, make it a TAMR boxcar. They are only \$5.00 postpaid and available from me; Jeff Wilke, 38115 Park Street, Oconomowoc, WI 53066.

Extra Board

Planning a regional convention or divisional meet and want as many TAMR members as possible to attend? If you are, then the EXTRA BOARD is where you want to announce your intentions to the whole membership. Please send all notices by the deadline listed on page 2 to the HOTBOX Editor.

On May 30, 31 & June 1, 1980, the Mid-Central Region of the NMRA will be holding a regional convention at the college of Steubenville in Steubenville, OH. Why are we telling you about an NMRA convention in the pages of the TAMR HOTBOX? Because a special registration area is being included for teenage modelers and all TAMR members who want to attend are welcome.

Since this is being held at a college, everything will be in one central location including lodging, if you so desire, plus meals. The organizers break down the possible costs for teens as follows: \$5.00 for registration, \$16.00 for lodging for two nights (2 per room) and \$16.00 minimum for four meals. Total cost \$37.00.

The program highlights include some of the NMRA's famous clinics presented by W. Allen McClelland, Dean Freytag and others. Layout tours by bus of the layouts in the Weirton-Steubenville area and movies (and slide shows) galore. There will also be model and photo contests and while TAMR members are not entitled to enter them (unless they have an NMRA membership), a separate table in the contest room could be set aside for TAMR models with possible judging by the NMRA judges (or TAMR members themselves).

You can get further information on the convention by writing: Joel Everly, 812 Broadway Blvd., Steubenville, OH 43952. As an added bonus, Lone Eagle Payne, our "Pivot Pin," says that he will be attending so plan to drop in and say hi!

SOLDERING

Soldering is an essential technique that every model railroader should know. Many times it is needed to make good electrical connections during the wiring phase of your layout. Now before I go into the details of how to solder (pronounced sah-der) correctly, I will explain about the kind of equipment you will need.

The most important item is, of course, the soldering iron or gun. The difference between the two is that the gun has a trigger and a light so that you can see your work. An iron, on the other hand, is nothing more than a heating tip with a handle attached which starts to heat when you plug it in. The advantage to using an iron instead of a gun is that it is not as bulky and so it is much easier to handle. Also, a gun will generally produce more heat than is needed and an iron has a tip which can be changed to one of a number of available sizes and shapes. Most irons come with a very cheap tip which may not last for long. The best thing to do is spend some money on a good tip rather than replacing the old kind all the time. A pencil, chisel or any other kind of pointed iron plated tip should work well in most model railroading applications.

Another item that you will obviously need is solder. There are two items which you should keep in mind when you purchase solder: the tin/lead content and flux. Solder with a ratio of 60/40 (60% tin, 40% lead) is the best kind of solder to get, although 50/50 (50% of each) could do in a pinch. Also, many solders today have what is called a resin or rosin core. Resin and rosin are types of flux. Flux is simply a substance which removes all oxidation on the metal before soldering. It can be purchased separately or it can be contained in the solder itself, it is best to get it already in the solder.

Before you can begin soldering, you must plug in your iron and let it heat up. Always be careful where

you put your iron, be careful not to touch the tip or lay it on plastic, etc. Always treat an iron as if it were hot because a cold iron looks just like a hot one. To determine when your iron is hot enough, touch a piece of solder lightly to the tip, if it melts, its hot. However, before you start soldering items in general, you should "tin" your iron. This is done by coating the tip of the iron with a small amount of solder and then wiping the excess off. Remaining will be a thin film of solder on the tip which will help prevent oxidation. It is also a good idea to tin wires and other components before you make the actual connections as this will make the job easier.

An important item to remember is that solder is used to make an electrical connection, not a mechanical one. Thus be sure that the items you are wiring are attached firmly to one another before you start soldering (a notable exception to this rule is the soldering of feeder wires to rail. Here it is impossible to make a mechanical connection so the solder serves as both an electrical and mechanical one which is the reason why care must be taken to get a good joint during this process).

Now, after you have made the mechanical connection, you should apply heat to the area you wish to solder. After a short time, take the solder and apply it to the other side of the area away from the iron. If the area is hot enough, the solder will melt and flow towards the heat. Once this has been accomplished take the solder away and then the iron, in that order. This is the proper way to solder, if it is done correctly, the solder will have a shiny appearance after it has cooled. If not, it will look dull and you'll have what it called a "cold joint" which will have to be done over. If you can not put the solder on the opposite side of the area, put it next to the iron, but do not melt the solder directly with the iron. If the solder will not melt, either the iron doesn't produce enough heat, it may have not had enough time to heat up;

(cont'd next page)

Soldering (cont'd from last page)

the tip needs to be cleaned (use steel wool) or the solder is of poor quality. If you remember the steps outlined below, you should get good results from your soldering without too much effort:

- 1) Get a good tip for your soldering iron.
- 2) Get good quality solder and check the tin/lead content (60/40 remember) and get resin or rosin core solder if possible.
- 3) Always be careful of your iron, a hot iron looks the same as a cold one.
- 4) Tin your iron and area you wish to solder before making a mechanical connection.
- 5) Make a good mechanical connection before applying solder.
- 6) Apply heat.
- 7) Apply solder to the opposite side or if not possible, next to the iron, but do not melt the solder directly with the iron.
- 8) Remove the solder when it starts to flow around the joint.
- 9) Remove the heat.
- 10) Check the joint. Make sure that it is not a cold joint and that it has a shiny appearance.

Getting To Know You

Any member, new or veteran, who has not already had his/her life story poured out to the world through these pages is invited to send an autobiography of about three paragraphs for publication. This is a reader oriented section which depends on you for support. We hope to include this column as often as there is material for it.

My name is CLAUDE MORELLI. I joined the TAMR last July after I saw an ad in MODEL RAILROADER. I live in Albuquerque, New Mexico. I am presently in the planning stages of my HO_{n3} Santa Fe Northwestern RR. When finished, the railroad will probably have about 170 feet of trackage, around 40 freight cars and six locomotives. The line's origin is derived from a real prototype with the same name which ran between Bernalillo, NM to the lumber rich areas around Porter, NM in the Jemez Mts. The prototype was a standard guage line, but I'm modeling it in narrow guage because I like to model in this guage. Also, I've extended my line another 35 miles to Chama, NM so that it could interchange with the famous narrow guage railroad there.

I am constructing the layout so that its era can be changed from

around 1940 to 1930. My railroad will interchange with the AT&SF at Bernalillo; the Santa Fe, San Juan and Northern (another prototype) at San Ysidri, the Gallagher Lumber Co. line at Porter and the D&RGW at Chama.

I am thirteen years old and have liked railroads all my life. I am a fan of the AT&SF, the Rio Grande Southern RR, the narrow lines of the D&RGW and the Nevada County Narrow Guage RR. In addition, I am also an NMRA member and belong to the Rocky Mountain region of the NMRA.

Hello, my name is DEE GILBERT and I am the chairman of the TAMR's Member Services Committee. I thought you may like to learn a few things about me.

My model railroading career started in 1954 with an American Flyer, 2 rail, S guage set. Then in the late 50's, I took my first shrinking step and converted to HO. In 1969, I took my last shrinking step, this time converting down to N scale.

In 1971, my trains were moved into a box and remained there for about five years. The locomotives and circle of track were occasionally brought out, lubricated, cleaned and ran--all amid day-dreams of grandure. In 1976, I turned my mind to doing more than think about boxed up trains. I brought them out and started a small layout in the back room of the office of the mobile home park that I manage. Within a week, I had enough track done for my wife to see what it was all about. From then on I had a whole room of trains while my wife, Linda, became the new equipment purchasing agent and Vice President of the L&D RR.

The L&D RR operates between Chicago and Seattle on a parallel to the Great Northern. We run our own full service passenger trains along with Amtrak, which has priority, and numerous freights.

Since beginning this layout four years ago, I've learned a great deal about N scale and this past summer a write up in MR about the TAMR opened up a new vista of enthusiasm. As a teenager in model railroading, I had no where to turn by my own imagination. So, in the TAMR, I find teenagers that were in my boat and I have a chance to help. Thus anyone wanting help can write me and make use of the MSC.

ELECTION 1980

To All Members:

First, I must say that the proposed Constitution and by-laws were approved by an overwhelming majority of those members who decided to cast ballots. Yet, I am a little disturbed by the small number of members who bothered to even return the ballot (just over 25%).

On the positive side, many of you who did return the ballots also submitted a number of nominations and herein appear the candidates who decided to accept them. I urge you to consider your choices wisely before casting your ballot. Take the time to read the biographical information of each candidate as well as his/her platform before marking your ballot. Five of these candidates will be put in charge by you of the livelihood of the TAMR; two on a national level, three on the regional.

Since this is the first time an election of this type has been held in the TAMR, I'd better explain the rules to you:

(1) ALL TAMR MEMBERS - Regular, Associate, Sustaining and Honorary - are entitled to cast ballots for the offices of Auditor and Treasurer.

(2) If you physically reside within the boundaries of the Canadian, Central or Northeastern Regions, you are also entitled to vote for your regional representative and your representative only. Thus those members living in the Southern, Western and International regions may not vote for a regional representative at this time (your chance comes next year).

(3) All those voting for a regional representative must specify their region in the space provided. This not only makes our job easier, but also helps in determining the eligibility of any write-in candidates.

(4) Improperly marked ballots and/or copies of the official ballot will be decalred void and will not be counted in the official tabulation. So be sure to mark your ballot PROPERLY.

Please take the time to vote--mailing instructions appear on the ballot proper --as your association now needs your help in determining how it will be run in the future.

-- Mark Kaszniak
TAMR Auditor

Auditor

Summary of duties: The Auditor is the head of the Constitutional Board; he is in charge of promoting the TAMR on national and local levels; he provides additional information on the TAMR to prospective members and he performs a yearly audit of the TAMR's finances.

GREG DAHL, 17, of St. Paul, MN has been a TAMR member for a couple of years. He is a frequent contributor to the HOTBOX and a member of the MESS. He is presently building an HO railroad, the "Minnesota Southern," and favors diesel and freight equipment.

"Right now the TAMR is in a rebuilding stage of its history. We need to work on many things and I want to strengthen the TAMR as much as possible."

"One of the things that I will accomplish if elected is to get more advertising in the model railroad press. The best way to get more members is to advertise ourselves by telling the teens out there that we want and can help them. We have ads in the two leading model magazines--and we all appreciate this--but there are many more magazines both here in the U.S. and countries abroad that we could advertise the TAMR. As of the present, we have 200+ members and both you and I know that there are more model railroad teens out there than that. My goal is to recruit those teens as members."

"I also know what it is like to have a minimal amount of money for spending on my hobby and thus I will work to cut down the amount of money that the officers spend and maybe we can direct more of our valuable funds to other things."

"Another one of my goals is a larger HOTBOX. We've got to get more people active in the TAMR. We've got to help our members more so that they'll renew with us. We've got a large turnover rate and I want to cut this down."

"So with combined co-operation among our members and officers, I think that we can boost the benefits of the TAMR and create a better association for us all. An association that will put FUN into model railroading and that will be strong and play an active role in our hobby."

GARY GARDNER, 19, of Salt Lake City, UT
(cont'd)

AUDITOR (cont'd)

has been a TAMR member for about a year. He is currently President of the Western Region as well as Editor-Publisher of the region's newsletter, the DAY-LIGHT. He is constructing an HO RR, "Rio Grande - Cottonwood Division," and favors steam and passenger equipment.

NO PLATFORM SUBMITTED BY DEADLINE

KEVIN GARRETT, 16, of Pueblo, CO has been a TAMR member for about a year. He is currently constructing an HO RR, the "Golden RR," and he favors steam equipment.

"I want to run for Auditor because I enjoy the TAMR and want to do my part in building and improving the association. I will be working my hardest to get a bigger and better HOTBOX. I will also do my best in getting the membership up and at the same time promote the hobby we all enjoy. So vote Kevin Garrett for Auditor."

JOHN VAN, 17, of Park Forest, IL has been a TAMR member for at least two years. He has written articles for both the HOTBOX and WAYFREIGHT and is a member of the MESS. He is in the process of constructing an HO RR, the "Chicago, Lake Michigan & Grand Haven," and favors steam, diesel, freight and passenger equipment.

"The TAMR is reaching a point in its history where our membership is beginning to increase. We are on the track to becoming a strong association and along with being strong in numbers, we must also be strong in administration. If elected Auditor, I will do my best to promote the TAMR on a national, as well as local, level."

"Some of my goals include bringing our membership up to a new high of 400 and adding more advertising and publicity for the benefit of the TAMR. I will make sure that prospective members are supplied with enough information to persuade them to join."

"We have a good association, but we need a strong administration in order to grow. Mark Kaszniak, our current Auditor, has done a superior job of keeping the TAMR in line and should be congratulated for his fine efforts. I hope to follow in his footsteps with your help. It's your association and what happens to it depends on you, so most of all--please vote!"

STEVE BOIVIN, 16, of Granby, MA has been a TAMR member for about a year. He is currently constructing an O scale RR, the "Chesapeake & Ohio," and favors diesel and electric equipment.

"Dear Friends: I'm running for the office of Auditor. I am a member of the Northeastern Region and just joined the Amherst Railway Society here in MA. I am a member of the Drones Bee club and belong to the Fox Hill riders snowmobile club. I love model railroading and promise to perform the duties of the office of Auditor to the best of my ability."

Treasurer

Summary of Duties: The Treasurer is to keep a record of all funds received and all funds paid out by the TAMR; he is to balance his books every six months and prepare a written financial report which shall be printed in the HOTBOX; he is to set up such depository bank accounts as he feels necessary for deposit of TAMR funds and is to make disbursements by check for expenditures on behalf of the association.

RICHARD HOKER, 16, of Glenview, IL has been a TAMR member for a couple of years and has written articles for the HOTBOX and the WAYFREIGHT. He is constructing an HO RR, "the Henley & Harlem Lines," and favors diesel and electric equipment.

"The office of Treasurer is a vital part of every association. Not only does he have to pay all the bills on time, but he also has to have an idea on how the rest of the year is going to go so that he can make plans to avoid any financial problems. The office of Treasurer should be as efficient as possible so that the money moves quickly and all our bills are paid on time. If I am elected, I hope to strive for these goals."

"We are all members of the only association of teens that have a common interest in model railroading. Because of this, we should elect people that have the ability and the will to keep this great group going. I have been a modeler for about five years and TAMR member for almost two years. As your Treasurer, I will keep you informed of the financial status of the TAMR so that you can see how we are doing. Now that you have read all the platforms, do something very important--Vote. It is vital that you show us who you want in office. It only takes a few minutes and a small 15¢ stamp. Thank you very much."

KESTUTIS RIMAS, of Lake Worth, FL has been a TAMR member less than a year. He is currently constructing an HO RR, the "Moon Valley," and he favors diesel and freight equipment.

NO PLATFORM SUBMITTED BY DEADLINE

RICHARD SONOSKI, 16, of Port Carbon, PA has been a TAMR member for about two years. He is currently the HOTBOX's advertising manager and a member of the Northeastern and Central Regions. He is constructing an HO RR and favors diesel and freight equipment.

"I have accepted the nomination for the office of Treasurer because I believe that I can help solve the problems plaguing this association. If I'm elected, I plan to start a nationwide membership drive--with your help--to increase our membership to 500."

J.S. WARD, 16, of Ruffsedale, PA has been a TAMR member for about two years. He has written articles for the HOTBOX and regional newsletters. He is constructing an N RR, the "Alleghany Rwy," and favors diesel and freight equipment.

"I feel that the TAMR is a fine thing and badly needed. So I wish to help out in an area where I can do well. Being somewhat of a math whiz, I believe that I can handle the job of Treasurer competently."

"Although Ray Hakim has done a fine job as treasurer, I have some ideas that will boost our treasury even more. If we have more money, we might be able to get bigger and more frequent HOTBOXs."

"Besides, getting involved in the association is something I have wanted to do since I joined. By this example, I hope that other TAMR members will get involved so that we will really be 'going places'."

"Yet no matter how you vote in this election, please remember to do so. Remember its your association, make it work for you!"

Regional Reps.

Summary of Duties: The regional representative is to maintain a liason between the TAMR and the region; he is to transmit such recommendations as are made by the regional organization; he is to provide information on regional activities to interested and/or prospective TAMR members upon request and he is to act as chief executive officer of the region

Canadian Rep.

TIM CANFIELD, 15, of Victoria, British Columbia has been a TAMR member for about a year. He is currently the appointed Secretary of the Canadian Region and is constructing an HO RR, the "Pacific Great Eastern." He favors diesel, traction and freight equipment.

"The Canadian Region has been unorganized for several years. Yet from the response obtained from a newsletter published last October, the "Snowplow," it is obvious that this region's members are enthusiastic about its revival."

"If elected the regional representative, I shall devote the required time and energy for the rebirth of the region and shall make it one of the most active in the most active in the TAMR."

BOB HERON, 18, of Brampton, Ontario has been a TAMR member for less than a year. He has been a contributor to the HOTBOX and is constructing an HO RR, the "Canadian National." He favors steam, diesel, freight and passenger equipment.

NO PLATFORM SUBMITTED BY DEADLINE

Central Rep.

CORBY ANDERSON, of Kenosha, WI has been a TAMR member for about a year. He is constructing an HO RR, the "Southfork & Black River," and he favors steam equipment.

"Hi! My name is Corby J. Anderson and I'm running for the office of Central Region Representative. My plan of what I'd like to accomplish while in office is as follows:

(1) To promote the TAMR by becoming more involved in it myself.

(2) To better develop the modeling prospective of the hobby by helping fellow members.

(3) To establish a three-way communication system between you--the member, myself and the entire hierarchy of the TAMR."

"If elected, I intend to better develop and represent the greatest hobby in this country--model railroading!"

JIM FRENCH, of Dixon, IL has been a TAMR member for about a year as well as a contributor to the HOTBOX and Central Region's WAYFREIGHT. He is a member of the MESS and is constructing an HO RR, the "Peoria & Eastern."

"Although I have only been a member for about a year, I feel that this is a highly successful and valuable association for the teen modeler. I have been notified that the Central Region is in need of a regional representative and I would like to fill that position."

"If elected, I would work to the best of my ability to acheive a higher membership for the Central Region and to encourage all our present members to renew their memberships. In return, more members means bigger and better HOTBOX's and WAYFREIGHT's."

"In closing. I ask yo to please vote for your regional representative so that we may acheive a higher membership and further the stability of our region and the TAMR."

Northeastern Rep.

TED BEDELL, 18, of Bayville, NY has been a TAMR member for a couple of years. He has written articles for both the HOTBOX and the NR's DEPOT and is current President of the Northeastern region. He is constructing an HO RR, the "Tonica & Santa Fe," and he favors diesel, freight and passenger equipment.

NO PLATFORM SUBMITTED BY DEADLINE

PAT LEWANDOWSKI, 18, of Wilmington, DE has been a TAMR member for about a year. He is constructing an HO RR, "Conrail--N.E. Division," and favors diesel equipment."

NO PLATFORM SUBMITTED BY DEADLINE

TED TAIT, 16, of Syosset, NY has been a TAMR member for a couple of years. He has written articles for both the HOTBOX and the NR's DEPOT and currently is Secretary-Treasurer of the NR as well as editor of the DEPOT. He is constructing an HO RR, the "Bedford & Orleans," and favors diesel and freight equipment.

NO PLATFORM SUBMITTED BY DEADLINE

A Brief History By Jim Kobrinetz

In The Noble Tradition

The heyday of the passenger train in America was marked by the great fleets of passenger trains. The Hiawatha's, the 400's, the Easgle's; all at one time epitomized all that was optimistic and forward looking about the railroads. The reigning monarch of these fleets was the Zephyr fleet of the Burlington. It was inaugurated in 1934 when the little Pioneer Zephyr's 14 hour run between Chicago and Denver thrilled the nation and soon the name Zephyr and speed became synonymous.

On the Chicago - San Francisco run, one streamlined train existed and it was the famous City of San Francisco of the UP-SP-C&NW. This train was fast and popular among businessmen. So much so that the competing Exposition Flyer of the CB&Q-D&RGW-WP just couldn't keep up even though it transversed a more scenic route. In August of 1943, officials of the three owner railroads of the Flyer met to discuss what would eventually become the California Zephyr. This new train would have everything: copious accommodations for first class passengers from drawing rooms (with showers) and bedrooms to roomettes and sections. It would also have several lounges including one in a rounded end observation car. For the coach passenger, lounges would also be available along with the last word in

coach accommodations. The dining cars would be epicurian delights and the entire train would gleam in a shining stainless steel envelope, courtesy of Budd. Finally, it would have the latest innovation in passenger cars--the dome--which would make the entire dream possible.

The California Zephyr was inaugurated in 1948 with five of these domes. It soon became a very profitable train for the three railroads involved and continued as such into the sixties. It was during this period that the pleasant little arrangement began to fall apart. The train began to lose money and the Western Pacific--the poorest of the three railroads--wanted out. However, by this time, the train had gained a certain mystique about it because it had maintained a high level of service through the sixties while railroads all around were rapidly diminishing their once proud passenger fleets. The train thus gained a small, but very loyal and vocal, group of admirers. This group consisted of both fans and people who enjoyed riding the train believing it the best way to see the country. They blocked the dropping of the train by the WP several times with court orders for they wanted to maintain the California Zephyr as a monument to what train travel once was. Finally in March of 1970, economics won out, possibly saving the WP from bankruptcy.

On March 23, 1970, the Rio Grande Zephyr was born. The CB&Q and D&RGW still ran the train from Chicago to Salt Lake City, but the CB&Q (now BN) merely called the train its California Service. The Rio Grande was the only one of the three to carry on the noble tradition. It only ran tri-weekly and its consist was somewhat abbreviated, but the fabulous Budd cars still ran, including the dome coaches and the markers were still brought up by the dome-lounge-observation that made the train famous.

On May 1, 1971, a new Zephyr was born. This train was the fledging Amtrak's new San Francisco Zephyr, a combination of BN's California service and the City of San Francisco. The train was expected to be routed by way of the Rio Grande, but at the last minute the D&RGW pulled out of Amtrak. The Rio Grande Zephyr would continue running.

The new San Francisco Zephyr turned out to be a Zephyr in name only. The trademark Budd cars were mixed liberally with other builders, many of which had smooth, painted sides. This unaesthetic combination compared poorly across the platforms in Denver with the Rio Grande Zephyr which maintained the uniform trainset and high standards of service of its predecessor.

In 1976, the sleepers and other first class accommodations were dropped from the RGZ when it switched to a daylight run. Naturally, the equipment has deteriorated somewhat, but the Rio Grande still maintains it well and with the dropping of the request from discontinuance, the Zephyr will undoubtedly continue to roll in the noble tradition.

BLUE MOUNTAIN RAILWAY

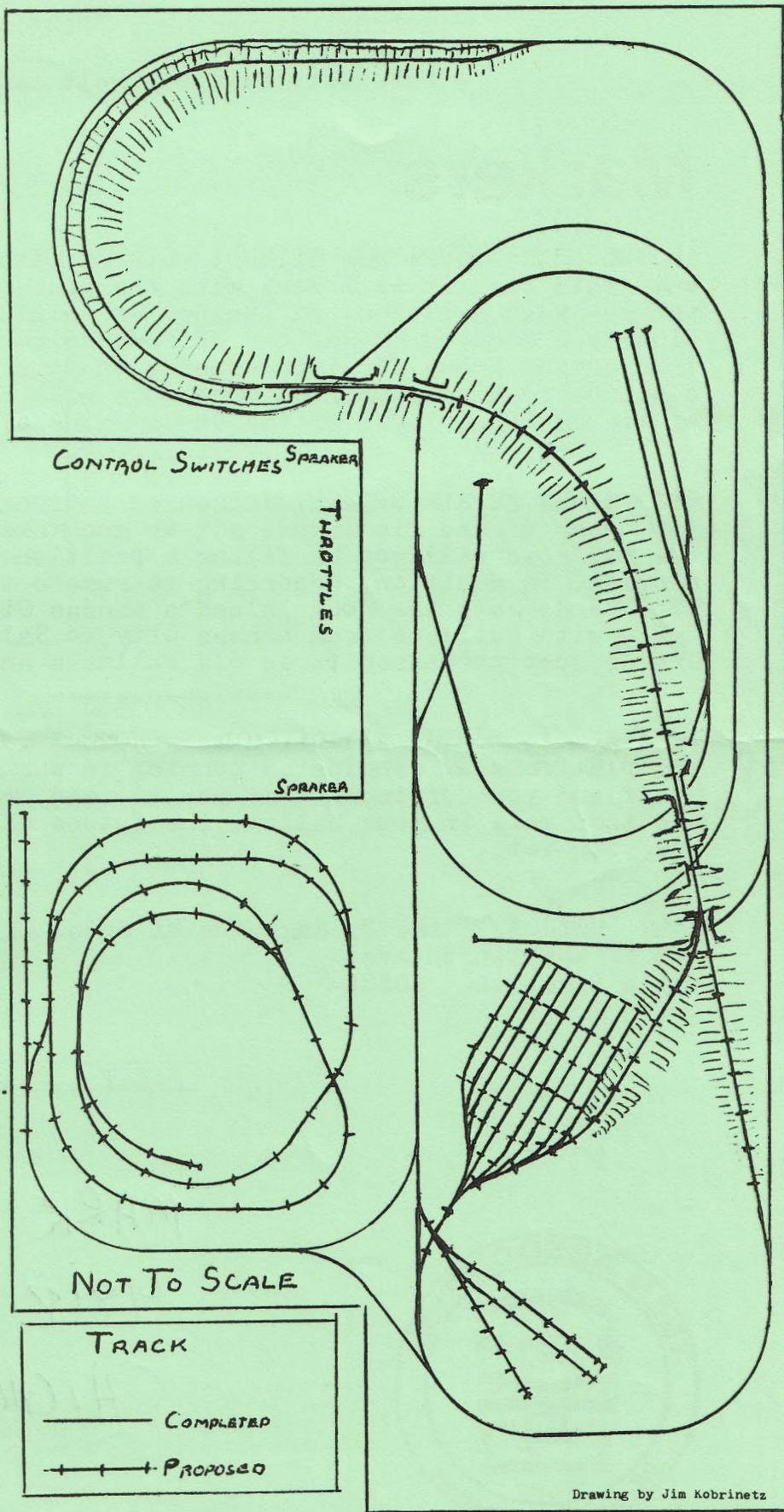
On Christmas 1977, my father gave me a Lionel Ontario Northland train set. At first I set it up and took it down every time I needed it, but soon I decided a permanent "home" was needed for the layout. Thus in January, I obtained a 4x8' board. A short time later, I went to the hobby shop and purchased a substantial amount of equipment and soon found out that a bigger layout was now needed. Once a larger table was procured, I began working on a suitable trackplan and then track laying began.

Next came wiring and although I had little experience and no books to guide me, I carefully thought out what I would need and then installed it. About this time I decided a multi level layout would be better than a single level one so I raised a portion of the track work (see plan) and then stapled wire screen and covered it with plaster. Once this was done, I colored the plaster to represent landscape.

As for ballast, I simply sprinkle fine sand between the rails and "fix it" with a solution of 6 parts water, 1 part white glue and 5 drops of dish soap applied through an eye dropper. When it dries, it is hard and looks loose. I then brushed away any loose sand that could get in my equipment.

Right now I'm in the scenery phase and am looking for some suitable posters to place on the walls to make my pike look more realistic.

So far, I haven't assembled very much equipment and my layout isn't as large as some of the others I'm seen, but I believe in sticking to my philosophy of going "slowly, but surely."



FRONT COVER: (TOP) F9A, #5571 leads the Rio Grande Zephyr through Midvale, UT.

(BOTTOM) Silver Sky, #1145, an ex-CZ dome lounge, is coupled to the rear of the Rio Grande Zephyr at Salt Lake City Station, Salt Lake City, UT.

Photos by Gary Gardner.

Markers:

IN THE NEXT ISSUE: Dee Gilbert returns with another derailment article only this time it will deal with the place that derailments most often occur--track switches. Ed Luzine, Jr. will give us a preview of his Liberty & Midland RR and our HELPERS column returns with some very interesting ideas for improvements on your pike. All this, the 1980 election results plus much much more will be contained in the consist of the May/June issue of the Un-Magazine of Model Railroading.

RIO GRANDE ZEPHYR UPDATE: Increased patronage on the RGZ has made the top brass of the Rio Grande sit up and take notice and as a result, the railroad will not be filing a petition this year to discontinue service. In addition, according to rumors that are circling:if the Rio Grande gets the Rock Island's Kansas City line, they will run a train with Pullmans from Kansas City to Salt Lake City as the Rio Grande just purchased three old Pullmans and a diner.

IT'S ELECTION TIME AGAIN: In this HOTBOX you will find a ballot plus the platforms of candidates running in our 1980 election. The HOTBOX Staff and your officers urge you to read over the platforms carefully and then mail in your ballot. The future of the TAMR depends on you and your vote.

TAMR HOTBOX, "the Un-Magazine of Model Railroading"
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